



**Matthew G. Bevin**  
Governor

**COMMONWEALTH OF KENTUCKY  
TRANSPORTATION CABINET**

Frankfort, Kentucky 40622  
[www.transportation.ky.gov/](http://www.transportation.ky.gov/)

**Greg Thomas**  
Secretary

December 4, 2018

CALL NO. 100  
CONTRACT ID NO. 181049  
ADDENDUM # 1

Subject: MCCRACKEN COUNTY, NHPP 0601(205)  
Letting December 7, 2018

- (1) Revised - Traffic Management Plan - Pages 17-28 of 121
- (2) Revised - Proposal Bid Items - Pages 117-121(a) of 121
- (3) Revised - Plan Sheets - R2, R2A, R2B, R2C, R2D, R2F, R2G, R2H, R2I, R2J, R2K, R2L, R2M, R2N, R69, R71, and R123

Proposal revisions are available at <http://transportation.ky.gov/Construction-Procurement/>.

If you have any questions, please contact us at 502-564-3500.

Sincerely,

A handwritten signature in black ink that reads "Rachel Mills".

Rachel Mills, P.E.  
Director  
Division of Construction Procurement

RM:mr  
Enclosures



An Equal Opportunity Employer M/F/D



Kentucky Transportation Cabinet  
Division of Highway Design  
**TRAFFIC MANAGEMENT PLAN**

County: McCracken Item No.: 01-154.00

Federal Project No.: NHPP 0601 182

**Project Description:**

Construct a Double-Crossover Diamond Interchange on US 60 at the I-24 Interchange near Kentucky Oaks Mall.

Roadway Classification:  Urban  Rural  
 Local  Collector  Arterial  Interstate

ADT (current) 36,000 AM Peak Current \_\_\_\_\_ PM Peak Current \_\_\_\_\_ % Trucks 10%

Project Designation:  Significant  Other: \_\_\_\_\_

**Traffic Control Plan Design:**

Taper and Diversion Design Speeds \_\_\_\_\_

Minimum Lane Width 11' Minimum Shoulder Width 2'

Minimum Bridge Width \_\_\_\_\_

Minimum Radius \_\_\_\_\_ Maximum Grade \_\_\_\_\_

Minimum Taper Length \_\_\_\_\_ Minimum Intersection Level of Service \_\_\_\_\_

Existing Traffic Queue Lengths \_\_\_\_\_ Projected Traffic Queue Lengths \_\_\_\_\_

**Comments:**

This project is broken into five phases.

The MOT plans are hereby referenced for details of the phasing and any disincentives associated with proposed closures.



Kentucky Transportation Cabinet  
**Division of Highway Design**  
**TRAFFIC MANAGEMENT PLAN**

Item No. 1-154.00

**Discussion:**

<b>1) Public Information Plan</b>			
a) Prepare with assistance from <input checked="" type="checkbox"/> KYTC or <input type="checkbox"/> _____			
b) Identify Trip Generators	N/A	f) Railroad Involvement	N/A
c) Identify Types of Road Users	Referenced	g) Address Pedestrians, Bikes Mass Transit	Referenced
d) Public Information Message	Referenced	h) Address Timing, Frequency, Updates, Effectiveness of Plan	Referenced
e) Public Information Strategies to be used	Referenced	i) Police & Other Emergency Services	Referenced

Stakeholders

- Utility Companies
  - Comcast: 270-538-7405
  - Jackson Purchase Energy Corporation: 270-442-7321
  - Paducah Power System: 270-575-4000
  - Paducah Power System Fibernet: 270-575-4004
  - Paducah Water: 270-444-5560
  - The Paducah – McCracken County Joint Sewer Agency (JSA): 270-575-0056
  - AT&T: 270-444-5048
  - ATMOS Energy Corporation: 270-685-8095
- Government Agencies
  - McCracken County Judge Executive: 270-444-4707
  - McCracken County Public Schools: 270-538-4000
  - McCracken County Sheriff: 270-444-4719
  - Paducah-McCracken County Office of Emergency Management: 270-448-1530
  - Paducah Mayor: 270-444-8504
  - Paducah Police Department: 270-444-8548
  - Paducah Fire Department: 270-444-8521
  - Paducah Public Schools: 270-444-5600
  - Paducah Post Office Carrier Annex: 270-444-9447
  - West Paducah Post Office: 270-744-6673
  - West Kentucky Community and Technical College: 270-554-9200
  - Murray State University – Paducah Regional Campus: 800-669-7654
  - Kentucky State Police, Post 1, Mayfield: 270-856-3721



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**Division of Highway Design**  
**TRAFFIC MANAGEMENT PLAN**

- Local Businesses
  - Plaza Tire Service: 270-442-2828
  - Hobby Lobby: 270-444-9275
  - Ulta Beauty: 270-442-8089
  - Shoe Carnival: 270-443-1401
  - David's Bridal: 270-442-2442
  - Old Navy: 270-442-7137
  - Canter Chiropractic & Rehab: 270-442-0110
  - Lowe's: 270-441-7000
  - Easy Money Shoppe: 270-443-2999
  - Aldi: 855-955-2534
  - Pier 1 Imports: 270-442-3928
  - Red Lobster: 270-443-5804
  - Captain D's: 270-442-9750
  - Fazoli's: 270-443-1632
  - Chuck-E-Cheeze's: 270-442-5114
  - Olive Garden: 270-442-4190
  - Chong's: 270-442-1710
  - Wendy's: 270-444-7622
  - Logan's Roadhouse: 270-442-1939
  - Los Amigos: 270-575-3285
  - Advance Auto Parts: 270-442-7980
  - Sleep Number: 270-443-8200
  - VapePark: 270-575-0001
  - GameStop & Cricket: 270-442-3850
  - Murphy, USA: 270-442-6227
  - Walmart: 270-444-0066
  - Steak N Shake: 270-575-4846
  - Kentucky Oaks Mall: 270-444-0440
  - Motel 6 Paducah: 270-443-3672
  - Cracker Barrel Old Country Store: 270-443-9331
  - Hardee's: 270-443-3687
  - Regions Bank: 800-734-4667
  - Sweet Cece's: 270-415-0409
  - Pet Smart: 270-575-9300
  - Books-A-Million: 270-443-6334
  - TJ Maxx: 270-444-8330
  - Office Depot: 270-575-9477
  - Brentley's Fine Cigars & Tobacco Barn: 270-442-7633
  - McAllister's Deli: 270-442-0203
  - AT&T: 270-444-0084
  - TGI Fridays: 270-415-9009
  - US Bank: 270-415-6942
  - Verizon: 270-444-9498
  - BP: 270-442-5296
  - Five Star Food Mart: 270-442-5296
  - Arby's: 270-443-7454



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**Division of Highway Design**  
**TRAFFIC MANAGEMENT PLAN**

- Plato's Closet Paducah: 270-575-0506
- Sleep Outfitters: 270-415-9229
- IHOP: 270-538-5244
- The Home Depot: 270-442-0817
- Taco Bell: 270-442-8226
- Pizza Hut: 270-443-9922
- Super 8 Paducah, Kentucky: 270-908-2866
- Panera Bread: 270-442-0948
- Pear Tree Inn Paducah: 270-444-7200
- Thrifty Inn Paducah: 270-444-0157
- Texas Roadhouse: 270-442-7000
- Buffalo Wild Wings: 270-444-5795
- Drury Suites Paducah: 270-441-0024
- Quality Suites: 270-442-2080
- Country Inn & Suites by Carlson, Paducah, KY: 270-442-2201
- McBride Mack Truck Sales: 270-442-7545
- Outback Steakhouse: 270-442-4111
- Drury Inn Paducah: 270-443-3313
- Bob Evans: 270-442-8700
- Applebee's Neighborhood Grill & Bar: 270-575-3675
- Auburn Place Hotel & Suites Paducah: 270-444-7667
- Holiday Inn Express & Suites Paducah West: 270-442-8874
- Rafferty's: 270-442-1014
- McDonald's: 270-444-2044
- Burger King: 270-444-9162

Local Media Outlets

- The Paducah Sun: 270-575-8600
- West Kentucky Star: 270-554-8255
- News Station WPSD-TV 6: 270-415-1900
- News Station WKPD-TV 41 (KET): 859-258-7244
- Radio Station WBEL 88.5: 662-844-8888
- Radio Station WGCF 89.3: 662-844-8888
- Radio Station WKYP 90.1: 800-525-5683
- Radio Station WKMS 91.3: 270-809-6793
- Radio Station W223BO (WKMS) 92.5: 270-809-6793
- Radio Station WNFC 91.7: 800-408-8888
- Radio Station WKYX 94.3: 270-554-8255
- Radio Station WKYX 570: 270-554-8255
- Radio Station WKYQ 93.3: 270-444-6397
- Radio Station WZYK 94.7: 270-444-6397
- Radio Station WDDJ 96.9: 270-444-6397
- Radio Station WPAD 1560: 270-444-6397
- Radio Station W258AN (WPAD-AM) 99.5: 270-444-6397
- Radio Station WLLE 102.1: 270-444-6397
- Radio Station WJLI 98.3: 270-442-0098



Kentucky Transportation Cabinet  
**Division of Highway Design**  
**TRAFFIC MANAGEMENT PLAN**

- Radio Station WCBL 99.1: 270-527-3102
- Radio Station WFGS 103.7: 270-753-2400
- Radio Station WREZ 105.5: 270-538-5251
- Radio Station W266CO (WREZ) 101.1: 270-538-5251
- Radio Station WZZL 106.7: 270-415-0599
- Radio Station WVPK (LPFM) 107.7: 270-201-2851
- Radio Station WRIK 750: 618-564-2171
- Radio Station W248CP (WRIK-AM) 97.5: 618-564-2171
- Radio Station W277CH (WRIK-AM) 103.3: 618-564-2171
- Radio Station WMOK 920: 618-524-4400
- Radio Station W284CG (WMOK-AM) 104.7: 618-524-4400
- Radio Station WCBL 1290: 270-527-3102
- Radio Station WDXR 1450: 270-554-8255
- Radio Station W273CH (WDXR-AM) 102.5: 270-554-8255

Prior to Construction

- The contractor will prepare and submit at the preconstruction conference a detailed traffic management plan to the engineer for review and approval. This plan will include, but not be limited to: a public information plan to be implemented before and during construction; maintenance of traffic procedures and signage; flagging and traffic control personnel and equipment; debris clean-up crews and equipment; construction equipment to be used on and around road work; passage or restriction of wide loads; and safety of traffic and construction personnel.
- KYTC District 1 will initiate contact with all stakeholders after the letting and prior to the beginning of construction to inform them of the time the construction will begin, the expected times and dates of roadway and lane closures, and any other anticipated impacts to travel and access. This contact is to be made sufficiently ahead of time to allow each stakeholder time to adjust to the changes.
- A public information campaign, utilizing local media outlets, portable changeable message boards, and other methods will be made to inform the traveling public at large of the impending construction. The information should include: anticipated lane closures, roadway closures, and the dates and times they are expected.
- Anticipated times of lane restrictions and total closures should be adjusted, if necessary, to accommodate special needs of the stakeholders or public at large.

During Construction

- The public information campaign will continue, using the same methods as prior to construction. Updates to travel impacts will be made, including those times which no closures are anticipated (such as periods of construction inactivity and holidays).
- A contact name and number will be provided to all identified stakeholders to allow for individual updates and information during regular business hours. A 24-hour, 7-days a week name and number will also be made available for contact in emergency situations.
- The Engineer and contractor will regularly review both the public information campaign and maintenance of traffic plan to ensure the needs and safety of the public are being met. This would include both method and timing of traffic management procedures.



Kentucky Transportation Cabinet  
**Division of Highway Design**  
**TRAFFIC MANAGEMENT PLAN**

- In addition to the normal placement of signs, variable message boards should be placed well in advance of the project to forewarn long-distance travelers who may not have had advance warning through local media.



Kentucky Transportation Cabinet  
Division of Highway Design  
**TRAFFIC MANAGEMENT PLAN**

Item No. 1-154.00

<b>2) Temporary Traffic Control Plan (For Each Phase of Construction)</b>	
<b>Phase 1</b>	
<b>Exposure Control Measures</b>	<b>Positive Protection Measures</b>
a) Is Road Closure Allowed Type: N/A	a) Address Drop Off Protection Criteria Referenced
b) Detour Conditions N/A	b) Temporary Barrier Requirements Referenced
c) Working Hour Restrictions Referenced	c) Evaluation of Existing Guardrail Conditions Referenced
d) Holiday or Special Event Work Restrictions Referenced	d) Address Temporary Drainage Referenced
e) Evaluation of Intersection LOS N/A	<b>Uniformed Law Enforcement Officers</b> N/A
f) Evaluation of Queue Lengths N/A	<b>Payment for Traffic Control*</b>
g) Evaluation of User Costs and Incentives/Disincentives N/A	a) Method of Project Bidding Referenced
h) Address Pedestrians, Bikes, Mass Transit Referenced	b) Special Notes Referenced
<b>Work Vehicles and Equipment</b> Referenced	*Payment for traffic control items shall be in accordance with the Kentucky Department of Highways Standard Specifications for Road and Bridge Construction
<b>Comments:</b>	
<p>Remove existing shoulders and construct new drainage, curb and gutter, and pavement on the outside of US 60 in each direction west of James Sanders Blvd.</p> <p>Complete construction of raised median and island on James Sanders Blvd. first to allow for free-flow right movement from US 60.</p> <p>Reduce US 60 lane widths to allow for removal of existing shoulders and construction of new pavement on the outside in each direction.</p> <p>Alternate shoulder closures on the ramps to construct widening of the ramps during this phase.</p> <p>Construct all new pavement that will not conflict with maintaining existing traffic flows.</p> <p>Remove existing Ramp B and Ramp D raised islands and replace with temporary flush pavement to be used during future construction phases.</p>	





Kentucky Transportation Cabinet  
Division of Highway Design  
**TRAFFIC MANAGEMENT PLAN**

Item No. 1-154.00

<b>2) Temporary Traffic Control Plan (For Each Phase of Construction)</b>	
<b>Phase 2</b>	
<b>Exposure Control Measures</b>	<b>Positive Protection Measures</b>
a) Is Road Closure Allowed Type: N/A	a) Address Drop Off Protection Criteria Referenced
b) Detour Conditions N/A	b) Temporary Barrier Requirements Referenced
c) Working Hour Restrictions Referenced	c) Evaluation of Existing Guardrail Conditions Referenced
d) Holiday or Special Event Work Restrictions Referenced	d) Address Temporary Drainage Referenced
e) Evaluation of Intersection LOS N/A	<b>Uniformed Law Enforcement Officers</b> N/A
f) Evaluation of Queue Lengths N/A	<b>Payment for Traffic Control*</b>
g) Evaluation of User Costs and Incentives/Disincentives N/A	a) Method of Project Bidding Referenced
h) Address Pedestrians, Bikes, Mass Transit Referenced	b) Special Notes Referenced
<b>Work Vehicles and Equipment</b> Referenced	*Payment for traffic control items shall be in accordance with the Kentucky Department of Highways Standard Specifications for Road and Bridge Construction
<b>Comments:</b>	
<p>Remove existing raised islands from entrances along right side of US 60.</p> <p>Shift traffic through the interchange to the outside in each direction.</p> <p>First, remove the existing raised medians from Sta. 195+45 to Sta. 196+70 and from Sta. 203+90 to Sta. 205+05 and replace with proposed pavement.</p> <p>Temporary pavement may be required to maintain left turn movements at the ramps. Relocate existing signal heads, as needed, to align with shifted traffic flow.</p> <p>Complete construction of the proposed US 60 raised median.</p> <p>Complete construction of Ramp B (entrance ramp to westbound I-24) utilizing alternating lane closures. Complete construction of Ramps A and C (I-24 exit ramps) by closing ramps on separate weekends / nights. To minimize disruption to traffic operations in the corridor, ramp closures are limited to weekends (or at times at the discretion of the Engineer) beginning at 7:00</p>	



Kentucky Transportation Cabinet  
**Division of Highway Design**  
**TRAFFIC MANAGEMENT PLAN**

PM. Friday until the following Monday at 6:00 AM. Liquidated damages of \$5,000.00 / hour will be assessed until these items of work are complete and the ramps are reopened. Dates for the closures shall be approved by the Engineer.



Kentucky Transportation Cabinet  
Division of Highway Design  
**TRAFFIC MANAGEMENT PLAN**

Item No. 1-154.00

<b>2) Temporary Traffic Control Plan (For Each Phase of Construction)</b>	
<b>Phase 3</b>	
<b>Exposure Control Measures</b>	<b>Positive Protection Measures</b>
a) Is Road Closure Allowed Type: Referenced	a) Address Drop Off Protection Criteria Referenced
b) Detour Conditions N/A	b) Temporary Barrier Requirements Referenced
c) Working Hour Restrictions Referenced	c) Evaluation of Existing Guardrail Conditions Referenced
d) Holiday or Special Event Work Restrictions Referenced	d) Address Temporary Drainage Referenced
e) Evaluation of Intersection LOS N/A	<b>Uniformed Law Enforcement Officers</b> N/A
f) Evaluation of Queue Lengths N/A	<b>Payment for Traffic Control*</b>
g) Evaluation of User Costs and Incentives/Disincentives N/A	a) Method of Project Bidding Referenced
h) Address Pedestrians, Bikes, Mass Transit Referenced	b) Special Notes Referenced
<b>Work Vehicles and Equipment</b> Referenced	*Payment for traffic control items shall be in accordance with the Kentucky Department of Highways Standard Specifications for Road and Bridge Construction
<b>Comments:</b>	
<p>Shift US 60 traffic west of James Sanders Blvd. to previously constructed outside lanes and construct the raised barrier median on US 60 and paint the flush islands in the entrances along US 60.</p> <p>Close Ramp D (entrance ramp to eastbound I-24) for a period of one weekend to construct the remaining portions of the ramp. To minimize disruption to traffic operations in the corridor, ramp closures are limited to a weekend (or at times at the discretion of the Engineer) beginning at 7:00 PM Friday until the following Monday at 6:00 AM. Liquidated damages of \$5,000.00 / hour will be assessed until these items of work are complete and the ramp is reopened. Dates for the closure shall be approved by the Engineer.</p>	



Kentucky Transportation Cabinet  
Division of Highway Design  
**TRAFFIC MANAGEMENT PLAN**

Item No. 1-154.00

<b>2) Temporary Traffic Control Plan (For Each Phase of Construction)</b>	
<b>Phase 4</b>	
<b>Exposure Control Measures</b>	<b>Positive Protection Measures</b>
a) Is Road Closure Allowed Type: Referenced	a) Address Drop Off Protection Criteria Referenced
b) Detour Conditions N/A	b) Temporary Barrier Requirements Referenced
c) Working Hour Restrictions Referenced	c) Evaluation of Existing Guardrail Conditions Referenced
d) Holiday or Special Event Work Restrictions Referenced	d) Address Temporary Drainage Referenced
e) Evaluation of Intersection LOS N/A	<b>Uniformed Law Enforcement Officers</b> N/A
f) Evaluation of Queue Lengths N/A	<b>Payment for Traffic Control*</b>
g) Evaluation of User Costs and Incentives/Disincentives Referenced	a) Method of Project Bidding Referenced
h) Address Pedestrians, Bikes, Mass Transit Referenced	b) Special Notes Referenced
<b>Work Vehicles and Equipment</b> Referenced	*Payment for traffic control items shall be in accordance with the Kentucky Department of Highways Standard Specifications for Road and Bridge Construction
<b>Comments:</b>	
<p>This is the Phase of construction when traffic will be switched to the DCD configuration.</p> <p>During this Phase of construction, the contractor will be allocated one weekend to close US 60, between James Sanders and Coleman Road, to finalize construction. With proposed signalization operational, re-open the roadway and route traffic into the proposed patterns. Liquidated damages of \$5000.00 / hour will be assessed until these items of work are complete. Dates for the weekend closure shall be approved by the Engineer.</p> <p>During this period, install proposed traffic signals and complete temporary striping in final striping configuration. Once the proposed DCD traffic signals are operational, traffic shall be switched to the ultimate DCD configuration.</p> <p>Complete construction of the proposed ramp islands once traffic has been switched.</p>	



Kentucky Transportation Cabinet  
Division of Highway Design  
**TRAFFIC MANAGEMENT PLAN**

Item No. 1-154.00

<b>2) Temporary Traffic Control Plan (For Each Phase of Construction)</b>	
<b>Phase 5</b>	
<b>Exposure Control Measures</b>	<b>Positive Protection Measures</b>
a) Is Road Closure Allowed Type: N/A	a) Address Drop Off Protection Criteria Referenced
b) Detour Conditions N/A	b) Temporary Barrier Requirements Referenced
c) Working Hour Restrictions Referenced	c) Evaluation of Existing Guardrail Conditions Referenced
d) Holiday or Special Event Work Restrictions Referenced	d) Address Temporary Drainage Referenced
e) Evaluation of Intersection LOS N/A	<b>Uniformed Law Enforcement Officers</b> N/A
f) Evaluation of Queue Lengths N/A	<b>Payment for Traffic Control*</b>
g) Evaluation of User Costs and Incentives/Disincentives N/A	a) Method of Project Bidding Referenced
h) Address Pedestrians, Bikes, Mass Transit Referenced	b) Special Notes Referenced
<b>Work Vehicles and Equipment</b> Referenced	*Payment for traffic control items shall be in accordance with the Kentucky Department of Highways Standard Specifications for Road and Bridge Construction
Comments:	
Utilizing alternating lane closures, complete placement of asphalt surface and pavement overlay.	

**PROPOSAL BID ITEMS**

181049

Page 1 of 6

Report Date 12/4/18

**Section: 0001 - PAVING**

LINE	BID CODE	ALT	DESCRIPTION	QUANTITY	UNIT	UNIT PRIC	FP	AMOUNT
0010	00003		CRUSHED STONE BASE	15,942.00	TON		\$	
0020	00100		ASPHALT SEAL AGGREGATE	53.80	TON		\$	
0030	00103		ASPHALT SEAL COAT	6.47	TON		\$	
0040	00190		LEVELING & WEDGING PG64-22	1,132.00	TON		\$	
0050	00191		ASPHALT SCRATCH COURSE PG64-22	920.00	TON		\$	
0060	00212		CL2 ASPH BASE 1.00D PG64-22	183.00	TON		\$	
0070	00214		CL3 ASPH BASE 1.00D PG64-22	13,020.00	TON		\$	
0080	00221		CL2 ASPH BASE 0.75D PG64-22	343.00	TON		\$	
0090	00301		CL2 ASPH SURF 0.38D PG64-22	186.00	TON		\$	
0100	00326		CL3 ASPH SURF 0.50B PG76-22 (REVISED: 12-4-18)	4,222.00	TON		\$	
0110	00356		ASPHALT MATERIAL FOR TACK	21.49	TON		\$	
0120	02073		JPC PAVEMENT-9 IN	1,256.00	SQYD		\$	
0130	02677		ASPHALT PAVE MILLING & TEXTURING	754.00	TON		\$	

**Section: 0002 - ROADWAY**

LINE	BID CODE	ALT	DESCRIPTION	QUANTITY	UNIT	UNIT PRIC	FP	AMOUNT
0140	00078		CRUSHED AGGREGATE SIZE NO 2	1,758.00	TON		\$	
0150	01000		PERFORATED PIPE-4 IN	7,966.00	LF		\$	
0160	01010		NON-PERFORATED PIPE-4 IN	123.00	LF		\$	
0170	01024		PERF PIPE HEADWALL TY 2-4 IN	4.00	EACH		\$	
0180	01028		PERF PIPE HEADWALL TY 3-4 IN	3.00	EACH		\$	
0190	01032		PERF PIPE HEADWALL TY 4-4 IN	1.00	EACH		\$	
0200	01690		FLUME INLET TYPE 1	2.00	EACH		\$	
0210	01691		FLUME INLET TYPE 2	4.00	EACH		\$	
0220	01740		CORED HOLE DRAINAGE BOX CON-4 IN	4.00	EACH		\$	
0230	01811		STANDARD CURB AND GUTTER MOD	8,128.00	LF		\$	
0240	01875		STANDARD HEADER CURB	304.00	LF		\$	
0250	01919		STANDARD BARRIER MEDIAN TYPE 3	580.00	SQYD		\$	
0260	01921		STANDARD BARRIER MEDIAN TYPE 4	1,118.00	SQYD		\$	
0270	01982		DELINEATOR FOR GUARDRAIL MONO DIRECTIONAL WHITE	9.00	EACH		\$	
0280	01983		DELINEATOR FOR GUARDRAIL MONO DIRECTIONAL YELLOW	8.00	EACH		\$	
0285	02003		RELOCATE TEMP CONC BARRIER (ADDED: 12-4-18)	2,000.00	LF		\$	
0290	02012		BARRICADE-TYPE I	9.00	EACH		\$	
0300	02014		BARRICADE-TYPE III	7.00	EACH		\$	
0310	02091		REMOVE PAVEMENT	235.00	SQYD		\$	
0320	02159		TEMP DITCH	4,605.00	LF		\$	
0330	02160		CLEAN TEMP DITCH	13,815.00	LF		\$	
0340	02230		EMBANKMENT IN PLACE	14,985.00	CUYD		\$	
0350	02237		DITCHING	537.00	LF		\$	
0360	02242		WATER	87.00	MGAL		\$	
0370	02351		GUARDRAIL-STEEL W BEAM-S FACE	800.00	LF		\$	
0380	02367		GUARDRAIL END TREATMENT TYPE 1	1.00	EACH		\$	

**PROPOSAL BID ITEMS**

181049

Page 2 of 6

Report Date 12/4/18

LINE	BID CODE	ALT	DESCRIPTION	QUANTITY	UNIT	UNIT PRIC	FP	AMOUNT
0390	02369		GUARDRAIL END TREATMENT TYPE 2A	4.00	EACH		\$	
0400	02381		REMOVE GUARDRAIL	537.50	LF		\$	
0410	02383		REMOVE & RESET GUARDRAIL	50.00	LF		\$	
0420	02483		CHANNEL LINING CLASS II	284.00	TON		\$	
0430	02545		CLEARING AND GRUBBING 14.37 ACRES	1.00	LS		\$	
0440	02555		CONCRETE-CLASS B	30.00	CUYD		\$	
0450	02562		TEMPORARY SIGNS	414.00	SQFT		\$	
0460	02585		EDGE KEY	398.00	LF		\$	
0470	02587		HOOK BOLT WITH EXPAN ANCHOR	110.00	EACH		\$	
0480	02599		FABRIC-GEOTEXTILE TYPE IV	2,300.00	SQYD		\$	
0490	02600		FABRIC GEOTEXTILE TY IV FOR PIPE	12,965.00	SQYD	\$2.00	\$	\$25,930.00
0500	02613		HANDRAIL-TYPE A-3	73.00	LF		\$	
0510	02650		MAINTAIN & CONTROL TRAFFIC	1.00	LS		\$	
0520	02671		PORTABLE CHANGEABLE MESSAGE SIGN	4.00	EACH		\$	
0530	02676		MOBILIZATION FOR MILL & TEXT	1.00	LS		\$	
0540	02696		SHOULDER RUMBLE STRIPS	2,728.00	LF		\$	
0550	02701		TEMP SILT FENCE	4,605.00	LF		\$	
0560	02703		SILT TRAP TYPE A	14.00	EACH		\$	
0570	02704		SILT TRAP TYPE B	14.00	EACH		\$	
0580	02705		SILT TRAP TYPE C	14.00	EACH		\$	
0590	02706		CLEAN SILT TRAP TYPE A	84.00	EACH		\$	
0600	02707		CLEAN SILT TRAP TYPE B	84.00	EACH		\$	
0610	02708		CLEAN SILT TRAP TYPE C	84.00	EACH		\$	
0620	02720		SIDEWALK-4 IN CONCRETE	297.00	SQYD		\$	
0630	02726		STAKING	1.00	LS		\$	
0640	02775		ARROW PANEL	4.00	EACH		\$	
0645	02898		RELOCATE CRASH CUSHION (ADDED: 12-4-18)	4.00	EACH		\$	
0650	02998		MASONRY COATING	70.00	SQYD		\$	
0655	03171		CONCRETE BARRIER WALL TYPE 9T (ADDED: 12-4-18)	2,000.00	LF		\$	
0660	05950		EROSION CONTROL BLANKET	3,788.00	SQYD		\$	
0670	05952		TEMP MULCH	69,843.00	SQYD		\$	
0680	05953		TEMP SEEDING AND PROTECTION	6,984.00	SQYD		\$	
0690	05963		INITIAL FERTILIZER	2.40	TON		\$	
0700	05964		20-10-10 FERTILIZER	4.00	TON		\$	
0710	05985		SEEDING AND PROTECTION	69,843.00	SQYD		\$	
0720	05990		SODDING	8,142.00	SQYD		\$	
0730	05992		AGRICULTURAL LIMESTONE	48.00	TON		\$	
0740	06401		FLEXIBLE DELINEATOR POST-M/W	75.00	EACH		\$	
0750	06404		FLEXIBLE DELINEATOR POST-M/Y	75.00	EACH		\$	
0760	06510		PAVE STRIPING-TEMP PAINT-4 IN	66,940.00	LF		\$	
0770	06511		PAVE STRIPING-TEMP PAINT-6 IN	14,020.00	LF		\$	
0780	06514		PAVE STRIPING-PERM PAINT-4 IN	33,515.00	LF		\$	
0790	06515		PAVE STRIPING-PERM PAINT-6 IN	6,909.00	LF		\$	
0800	06516		PAVE STRIPING-PERM PAINT-8 IN	2,840.00	LF		\$	
0810	06517		PAVE STRIPING-PERM PAINT-12 IN	320.00	LF		\$	
0820	06566		PAVE MARKING-THERMO X-WALK-12 IN	472.00	LF		\$	
0830	06568		PAVE MARKING-THERMO STOP BAR-24IN	908.00	LF		\$	

**PROPOSAL BID ITEMS**

181049

Page 3 of 6

Report Date 12/4/18

LINE	BID CODE	ALT	DESCRIPTION	QUANTITY	UNIT	UNIT PRIC	FP	AMOUNT
0840	06570		PAVE MARKING-PAINT CROSS-HATCH	1,463.00	SQFT		\$	
0850	06572		PAVE MARKING-DOTTED LANE EXTEN	683.00	LF		\$	
0860	06573		PAVE MARKING-THERMO STR ARROW	49.00	EACH		\$	
0870	06574		PAVE MARKING-THERMO CURV ARROW	100.00	EACH		\$	
0880	06575		PAVE MARKING-THERMO COMB ARROW	36.00	EACH		\$	
0890	06576		PAVE MARKING-THERMO ONLY	7.00	EACH		\$	
0900	06583		PAVEMENT MARKER TYPE IV-B W/R	85.00	EACH		\$	
0910	06584		PAVEMENT MARKER TYPE IV-B Y/R	181.00	EACH		\$	
0920	06600		REMOVE PAVEMENT MARKER TYPE V	544.00	EACH		\$	
0930	08001		STRUCTURE EXCAVATION-COMMON	37.00	CUYD		\$	
0940	08100		CONCRETE-CLASS A	414.64	CUYD		\$	
0950	08150		STEEL REINFORCEMENT	509.00	LB		\$	
0960	08151		STEEL REINFORCEMENT-EPOXY COATED	2,022.00	LB		\$	
0970	08902		CRASH CUSHION TY VI CLASS B TL3	2.00	EACH		\$	
0972	08903		CRASH CUSHION TY VI CLASS BT TL3 (ADDED: 12-4-18)	4.00	EACH		\$	
0980	10020NS		FUEL ADJUSTMENT	39,467.00	DOLL	\$1.00	\$	\$39,467.00
0990	10030NS		ASPHALT ADJUSTMENT	78,215.00	DOLL	\$1.00	\$	\$78,215.00
1000	20071EC		JOINT ADHESIVE	38,671.00	LF		\$	
1010	20099ES842		PAVE MARK TEMP PAINT STOP BAR	264.00	LF		\$	
1020	20191ED		OBJECT MARKER TY 3	1.00	EACH		\$	
1030	20430ED		SAW CUT	15,553.00	LF		\$	
1035	21800EN		BORE AND JACK PIPE-30 IN (ADDED: 12-4-18)	126.00	LF		\$	
1040	22520EN		PAVE MARKING-THERMO YIELD BAR-36 IN	36.00	LF		\$	
1050	22664EN		WATER BLASTING EXISTING STRIPE	24,000.00	LF		\$	
1060	22861EN		HIGH STRENGTH GEOTEXTILE FABRIC TY V	1,200.00	SQYD		\$	
1070	23143ED		KPDES PERMIT AND TEMP EROSION CONTROL	1.00	LS		\$	
1080	23158ES505		DETECTABLE WARNINGS	274.00	SQFT		\$	
1090	23274EN11F		TURF REINFORCEMENT MAT 1	846.00	SQYD		\$	
1100	23791EC		PAVE STRIPING-CHEVRON MARKINGS	345.00	SQFT		\$	
1110	24109EC		BARRIER CURB AND GUTTER-MOD	5,712.00	LF		\$	
1120	24110EC		PERM PAINT-BARRIER CURB	5,633.00	LF		\$	
1130	24489EC		INLAID PAVEMENT MARKER	921.00	EACH		\$	
1140	24631EC		BARCODE SIGN INVENTORY	105.00	EACH		\$	
1150	24640ED		OBJECT MARKER TYPE 1	2.00	EACH		\$	
1160	24814EC		PIPELINE INSPECTION	4,024.00	LF		\$	

**Section: 0003 - DRAINAGE**

LINE	BID CODE	ALT	DESCRIPTION	QUANTITY	UNIT	UNIT PRIC	FP	AMOUNT
1170	00461		CULVERT PIPE-15 IN	235.00	LF		\$	
1180	00462		CULVERT PIPE-18 IN	111.00	LF		\$	
1190	00464		CULVERT PIPE-24 IN	7.00	LF		\$	
1200	00521		STORM SEWER PIPE-15 IN	1,770.00	LF		\$	
1210	00522		STORM SEWER PIPE-18 IN	1,262.00	LF		\$	
1220	00524		STORM SEWER PIPE-24 IN	802.00	LF		\$	
1230	00526		STORM SEWER PIPE-30 IN	177.00	LF		\$	



**PROPOSAL BID ITEMS**

181049

Page 4 of 6

Report Date 12/4/18

LINE	BID CODE	ALT	DESCRIPTION	QUANTITY	UNIT	UNIT PRIC	FP	AMOUNT
1240	01202		PIPE CULVERT HEADWALL-15 IN	12.00	EACH		\$	
1250	01204		PIPE CULVERT HEADWALL-18 IN	3.00	EACH		\$	
1260	01208		PIPE CULVERT HEADWALL-24 IN	4.00	EACH		\$	
1270	01210		PIPE CULVERT HEADWALL-30 IN	1.00	EACH		\$	
1280	01432		SLOPED BOX OUTLET TYPE 1-15 IN	3.00	EACH		\$	
1290	01433		SLOPED BOX OUTLET TYPE 1-18 IN	4.00	EACH		\$	
1300	01434		SLOPED BOX OUTLET TYPE 1-24 IN	1.00	EACH		\$	
1310	01456		CURB BOX INLET TYPE A	38.00	EACH		\$	
1320	01459		CURB BOX INLET TYPE A MOD	11.00	EACH		\$	
1330	01480		CURB BOX INLET TYPE B	2.00	EACH		\$	
1340	01490		DROP BOX INLET TYPE 1	2.00	EACH		\$	
1350	01634		CAP CURB BOX INLET	4.00	EACH		\$	
1360	01650		JUNCTION BOX	3.00	EACH		\$	
1370	01719		ADJUST INLET	1.00	EACH		\$	
1380	01791		ADJUST MANHOLE FRAME TO GRADE	7.00	EACH		\$	
1390	01792		ADJUST MANHOLE	2.00	EACH		\$	
1400	02402		REMOVE STONE MASONRY	.73	CUYD		\$	
1410	03000		PRECAST CONC BOX SECT 13'X3'	20.00	LF		\$	
1420	08100		CONCRETE-CLASS A	2.67	CUYD		\$	
1430	08103		CONCRETE-CLASS D MOD	.73	CUYD		\$	

**Section: 0004 - SIGNING**

LINE	BID CODE	ALT	DESCRIPTION	QUANTITY	UNIT	UNIT PRIC	FP	AMOUNT
1440	06405		SBM ALUMINUM PANEL SIGNS	1,122.00	SQFT		\$	
1450	06406		SBM ALUM SHEET SIGNS .080 IN	456.00	SQFT		\$	
1460	06407		SBM ALUM SHEET SIGNS .125 IN	270.00	SQFT		\$	
1470	06410		STEEL POST TYPE 1	1,314.00	LF		\$	
1480	06412		STEEL POST MILE MARKERS	1.00	EACH		\$	
1490	06441		GMSS GALV STEEL TYPE C	3,246.00	LB		\$	
1500	06448		SIGN BRIDGE ATTACHMENT BRACKET	2.00	EACH		\$	
1510	06490		CLASS A CONCRETE FOR SIGNS	11.50	CUYD		\$	
1520	06491		STEEL REINFORCEMENT FOR SIGNS	500.00	LB		\$	
1530	20418ED		REMOVE & RELOCATE SIGNS	8.00	EACH		\$	
1540	21373ND		REMOVE SIGN	50.00	EACH		\$	
1550	21596ND		GMSS TYPE D	4.00	EACH		\$	

**Section: 0005 - SIGNALIZATION**

LINE	BID CODE	ALT	DESCRIPTION	QUANTITY	UNIT	UNIT PRIC	FP	AMOUNT
1560	04740		POLE BASE	3.00	EACH		\$	
1570	04780		FUSED CONNECTOR KIT	48.00	EACH		\$	
1580	04792		CONDUIT-1 IN	154.00	LF		\$	
1590	04795		CONDUIT-2 IN	1,830.00	LF		\$	
1600	04811		ELECTRICAL JUNCTION BOX TYPE B	37.00	EACH		\$	
1610	04820		TRENCHING AND BACKFILLING	3,229.00	LF		\$	

**PROPOSAL BID ITEMS**

181049

Page 5 of 6

Report Date 12/4/18

LINE	BID CODE	ALT	DESCRIPTION	QUANTITY	UNIT	UNIT PRIC	FP	AMOUNT
1620	04830		LOOP WIRE	19,130.00	LF		\$	
1630	04834		WIRE-NO. 6	1,320.00	LF		\$	
1640	04835		WIRE-NO. 4	1,420.00	LF		\$	
1650	04844		CABLE-NO. 14/5C	16,881.00	LF		\$	
1660	04850		CABLE-NO. 14/1 PAIR	21,827.00	LF		\$	
1670	04871		POLE 35 FT WOODEN	1.00	EACH		\$	
1680	04881		MAST ARM POLE	7.00	EACH		\$	
1690	04885		MESSENGER-10800 LB	994.00	LF		\$	
1700	04886		MESSENGER-15400 LB	1,148.00	LF		\$	
1710	04895		LOOP SAW SLOT AND FILL	7,005.00	LF		\$	
1720	04932		INSTALL STEEL STRAIN POLE	16.00	EACH		\$	
1730	04934		TEMP SIGNAL MULTI PHASE	2.00	EACH		\$	
1740	06472		INSTALL SPAN MOUNTED SIGN	28.00	EACH		\$	
1750	20093NS835		INSTALL PEDESTRIAN HEAD-LED	12.00	EACH		\$	
1760	20094ES835		TEMP RELOCATION OF SIGNAL HEAD	102.00	EACH		\$	
1770	20188NS835		INSTALL LED SIGNAL-3 SECTION	69.00	EACH		\$	
1780	20266ES835		INSTALL LED SIGNAL- 4 SECTION	4.00	EACH		\$	
1790	20390NS835		INSTALL COORDINATING UNIT	5.00	EACH		\$	
1800	20391NS835		ELECTRICAL JUNCTION BOX TYPE A	7.00	EACH		\$	
1810	20392NS835		ELECTRICAL JUNCTION BOX TYPE C	4.00	EACH		\$	
1820	21743NN		INSTALL PEDESTRIAN DETECTOR	12.00	EACH		\$	
1830	22939ND		INSTALL LUMINAIRE POLE	3.00	EACH		\$	
1840	23157EN		TRAFFIC SIGNAL POLE BASE	94.90	CUYD		\$	
1850	23222EC		INSTALL SIGNAL PEDESTAL	8.00	EACH		\$	
1860	24856ED		TRAFFIC SIGNAL POLE BASE ALTERNATE	23.00	EACH		\$	
1870	24900EC		PVC CONDUIT-1 1/4 IN-SCHEDULE 80	1,138.00	LF		\$	
1880	24901EC		PVC CONDUIT-2 IN-SCHEDULE 80	2,712.00	LF		\$	
1890	24908EC		INSTALL SIGNAL CONTROLLER-TY ATC	5.00	EACH		\$	
1900	24955ED		REMOVE SIGNAL EQUIPMENT	6.00	EACH		\$	

**Section: 0006 - LIGHTING**

LINE	BID CODE	ALT	DESCRIPTION	QUANTITY	UNIT	UNIT PRIC	FP	AMOUNT
1910	04714		POLE 120 FT MTG HT HIGH MAST	7.00	EACH		\$	
1920	04761		LIGHTING CONTROL EQUIPMENT	1.00	EACH		\$	
1930	04797		CONDUIT-3 IN	2,325.00	LF		\$	
1940	04800		MARKER	14.00	EACH		\$	
1950	04820		TRENCHING AND BACKFILLING	4,490.00	LF		\$	
1960	04940		REMOVE LIGHTING	1.00	LS		\$	
1970	20391NS835		ELECTRICAL JUNCTION BOX TYPE A	12.00	EACH		\$	
1980	20392NS835		ELECTRICAL JUNCTION BOX TYPE C	2.00	EACH		\$	
1990	20410ED		MAINTAIN LIGHTING	1.00	LS		\$	
2000	21543EN		BORE AND JACK CONDUIT	2,325.00	LF		\$	
2010	23161EN		POLE BASE-HIGH MAST	66.52	CUYD		\$	
2020	24749EC		HIGH MAST LED LUMINAIRE	34.00	EACH		\$	
2030	24851EC		CABLE-NO. 10/3C DUCTED	10,060.00	LF		\$	

181049

### PROPOSAL BID ITEMS

Report Date 12/4/18

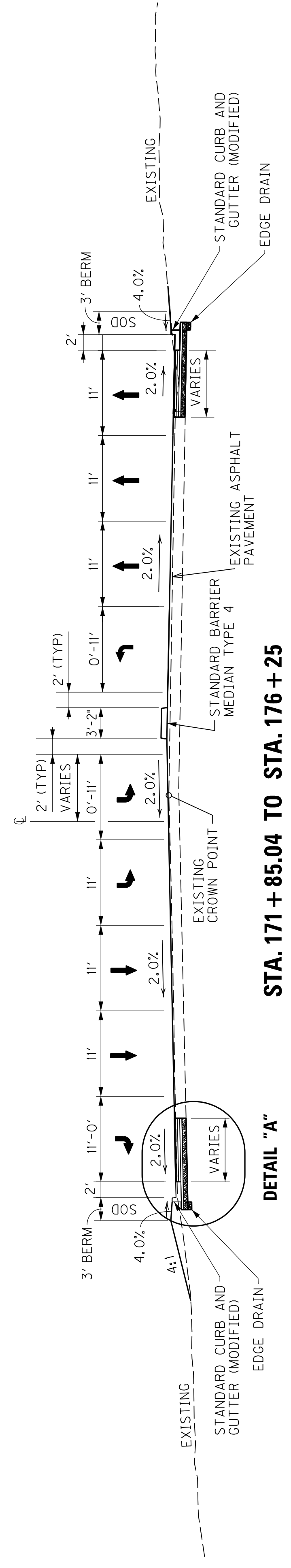
Page 6 of 6

#### Section: 0007 - MOBILIZATION AND/OR DEMOBILIZATION

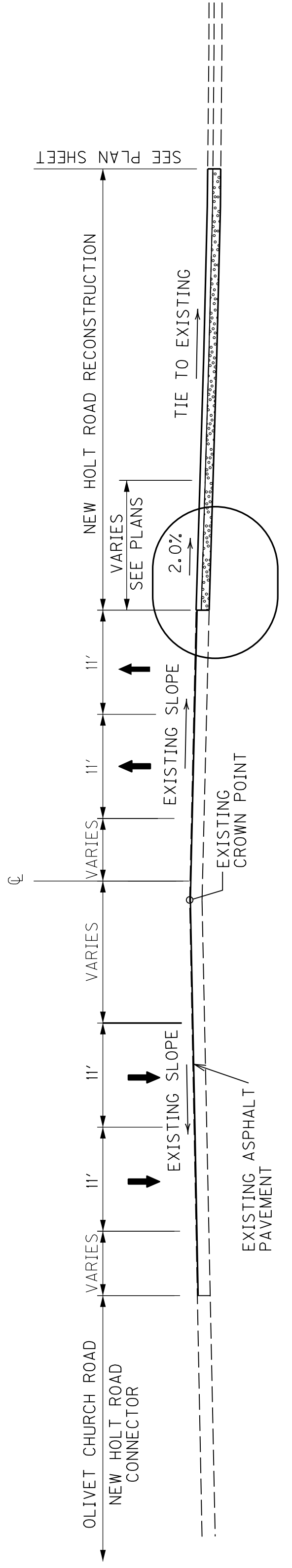
LINE	BID CODE	ALT	DESCRIPTION	QUANTITY	UNIT	UNIT PRIC	FP	AMOUNT
2040	02568		MOBILIZATION	1.00	LS		\$	
2050	02569		DEMOBILIZATION	1.00	LS		\$	

# TYPICAL SECTIONS US 60

COUNTY OF	ITEM NO.	SHEET NO.
McCRACKEN	I-154.00	R2

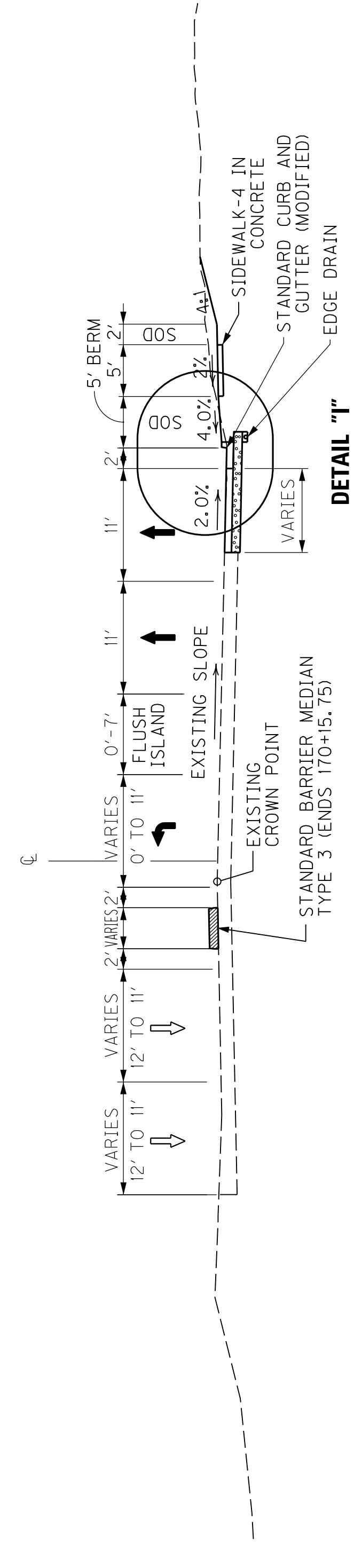


**STA. 171 + 85.04 TO STA. 176 + 25**  
**DETAIL "A"**



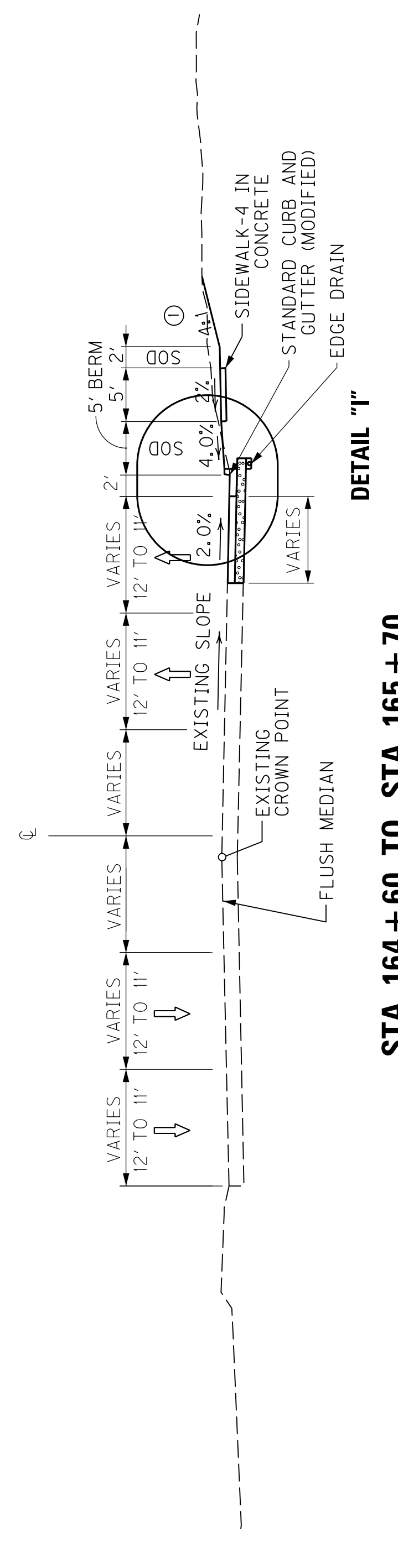
**DETAIL "J"**

**STA. 170 + 15.75 TO STA. 171 + 85.04**



**DETAIL "I"**

**STA. 165 + 70 TO STA. 170 + 15.75**



**DETAIL "I"**

**STA. 164 + 60 TO STA. 165 + 70**

**NEW CONSTRUCTION  
GRADE, DRAIN & SURFACING  
- USING -**

- US 60 PAVEMENT**
- 10.00" CRUSHED STONE BASE
- 4.00" CL3 ASPH. BASE 1.00D PG64-22
- 3.75" CL3 ASPH. BASE 1.00D PG64-22
- 3.75" CL3 ASPH. BASE 1.00D PG64-22
- 1.50" SURFACE - 1.50" CL3 ASPH. SURF. 0.50B PG76-22

**SHOULDERS:**  
VARIES: STANDARD CURB AND GUTTER (MODIFIED)  
BARRIER CURB AND GUTTER (MODIFIED) (SEE US 60 TYPICAL SECTION)

**PAVEMENT OVERLAY  
- USING -**

- US 60 PAVEMENT**
- LEVEL & WEDGE PG64-22
- 1.50" CL3 ASPH. SURF. 0.50B PG76-22

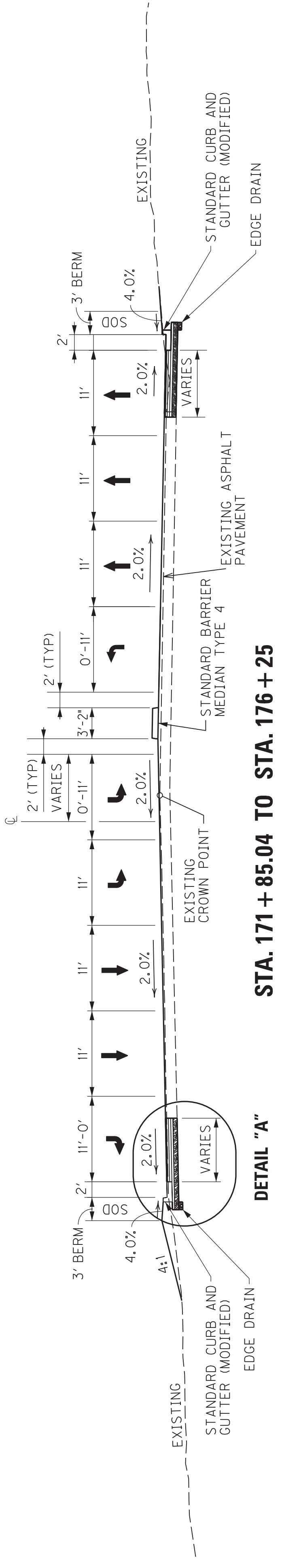
① SEE CROSS SECTIONS FOR CONSTRUCTION OUTSIDE THE SIDEWALK.

# TYPICAL SECTIONS

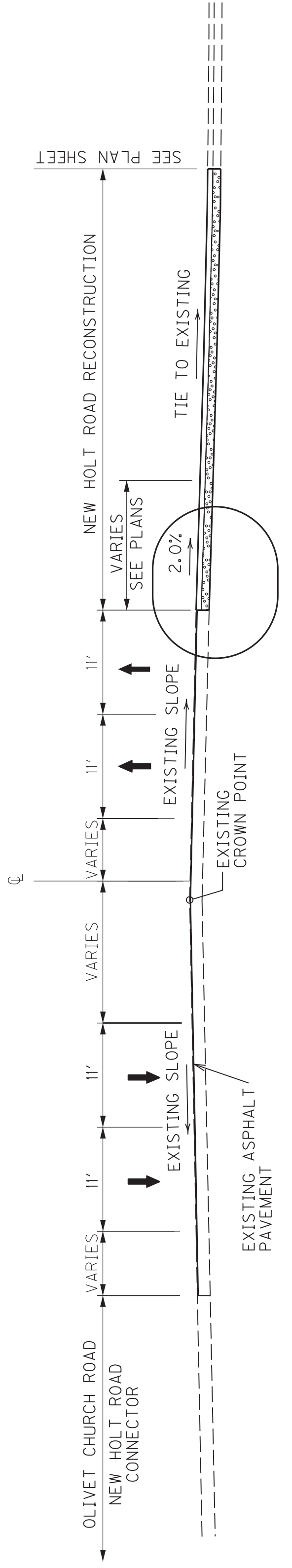
## US 60

COUNTY OF	ITEM NO.	SHEET NO.
McCRACKEN	I-154.00	R2

REVISED 11-30-18

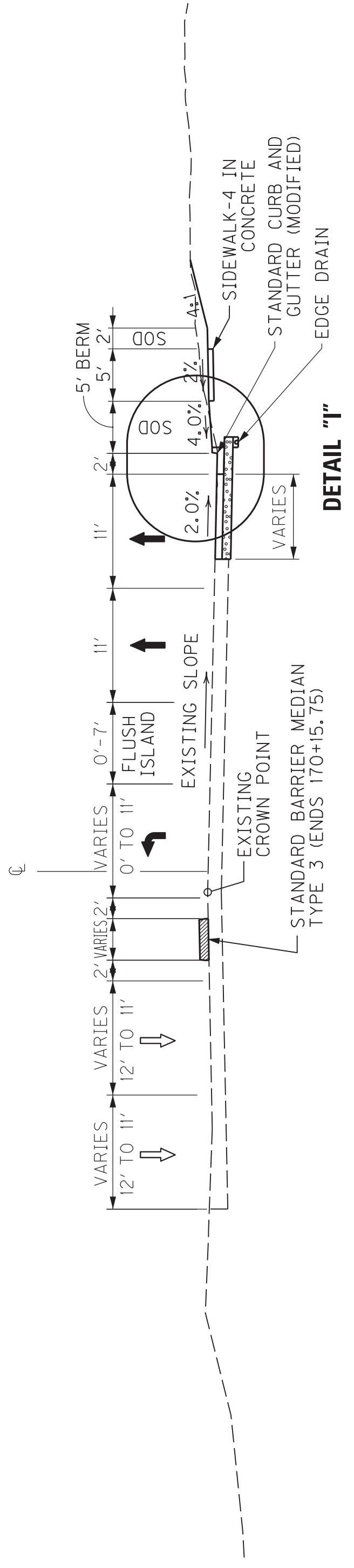


**STA. 171 + 85.04 TO STA. 176 + 25**



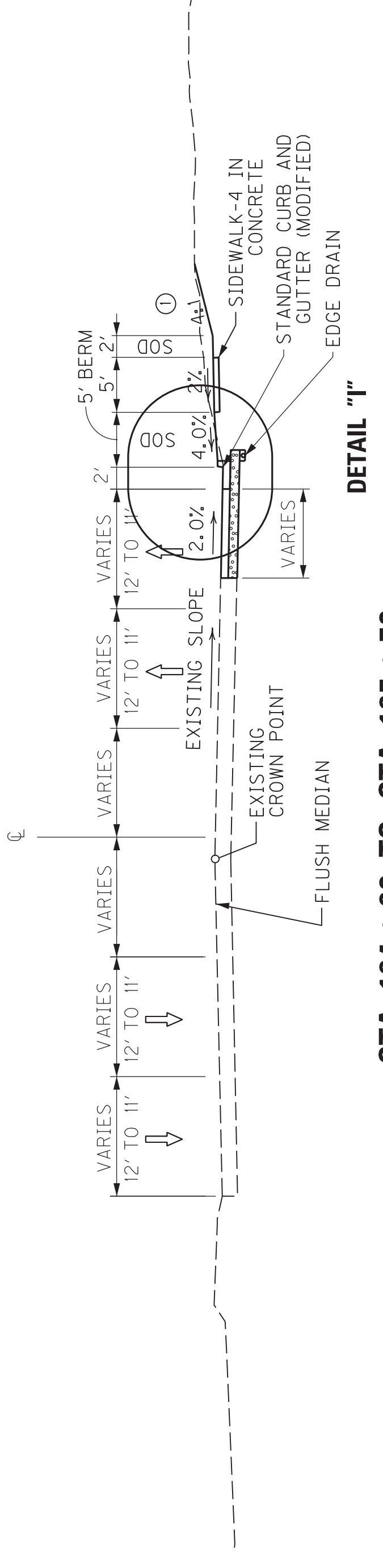
**DETAIL "J"**

**STA. 170 + 15.75 TO STA. 171 + 85.04**



**DETAIL "I"**

**STA. 165 + 70 TO STA. 170 + 15.75**



**DETAIL "I"**

**STA. 164 + 60 TO STA. 165 + 70**

NEW CONSTRUCTION  
GRADE, DRAIN & SURFACING  
- USING -

- US 60 PAVEMENT
- 10.00" CRUSHED STONE BASE
- 4.00" CL3 ASPH. BASE 1.00D PG64-22
- 3.75" CL3 ASPH. BASE 1.00D PG64-22
- 3.75" CL3 ASPH. BASE 1.00D PG64-22
- 1.50" CL3 ASPH. SURF. 0.50B PG76-22

VARIES: STANDARD CURB AND GUTTER (MODIFIED)  
BARRIER CURB AND GUTTER (MODIFIED) (SEE US 60 TYPICAL SECTION)

PAVEMENT OVERLAY  
- USING -

- US 60 PAVEMENT
- LEVEL 1 - WEAR - PG64-22
- 1.50" CL3 ASPH. SURF. 0.50B PG76-22

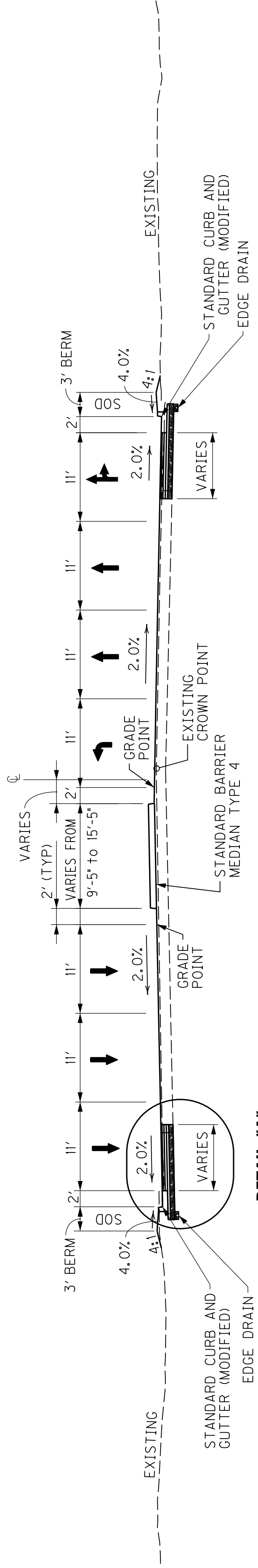
SEE CROSS SECTIONS FOR CONSTRUCTION OUTSIDE THE SIDEWALK.

SCALE: NTS

TYPICAL SECTIONS

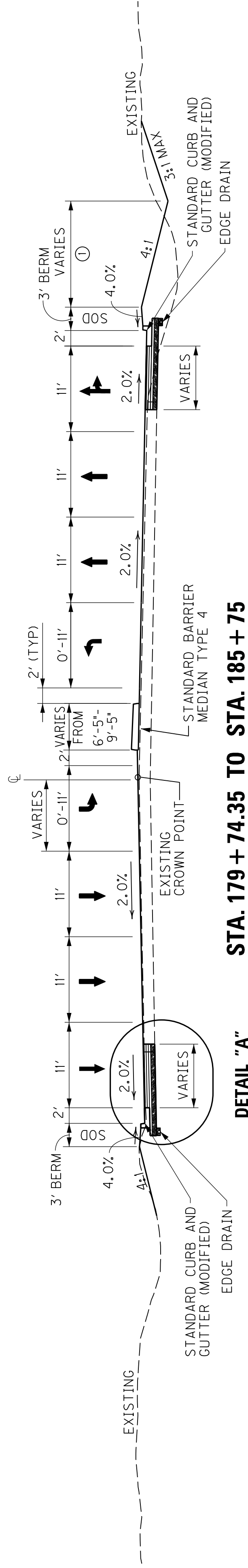
# TYPICAL SECTIONS US 60

COUNTY OF	ITEM NO.	SHEET NO.
McCRACKEN	I-154.00	R2A



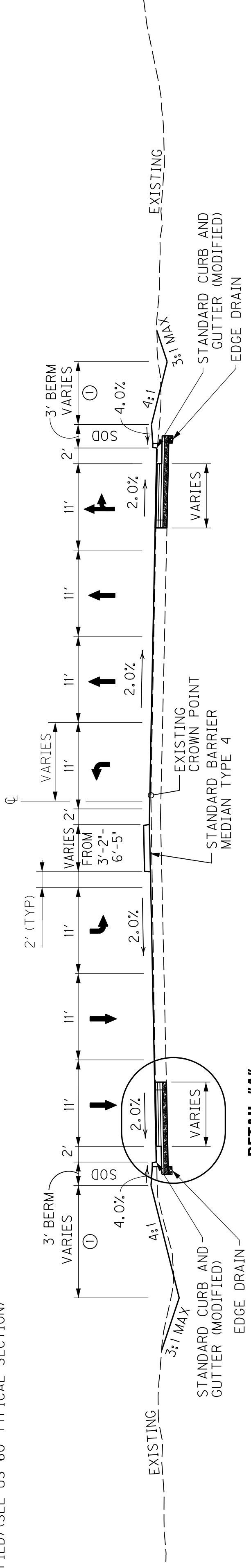
**STA. 185 + 75 TO STA. 188 + 98.31**

**DETAIL "A"**



**STA. 179 + 74.35 TO STA. 185 + 75**

**DETAIL "A"**



**STA. 176 + 25 TO STA. 179 + 74.35**

**DETAIL "A"**

**NEW CONSTRUCTION  
GRADE, DRAIN & SURFACING  
- USING -**

- US 60 PAVEMENT**
- 10.00" CRUSHED STONE BASE
- 4.00" CL3 ASPH. BASE 1,000 PG64-22
- 3.75" CL3 ASPH. BASE 1,000 PG64-22
- 3.75" CL3 ASPH. BASE 1,000 PG64-22
- 1.50" SURFACE - 1.50" CL3 ASPH. SURF. 0.50B PG76-22

**SHOULDERS**  
VARIES: STANDARD CURB AND GUTTER (MODIFIED)  
BARRIER CURB AND GUTTER (MODIFIED) (SEE US 60 TYPICAL SECTION)

**PAVEMENT OVERLAY  
- USING -  
US 60 PAVEMENT**

- LEVEL & WEDGE PG64-22
- 1.50" CL3 ASPH. SURF. 0.50B PG76-22

① SEE CROSS SECTIONS FOR DITCH OFFSETS AND ELEVATIONS.

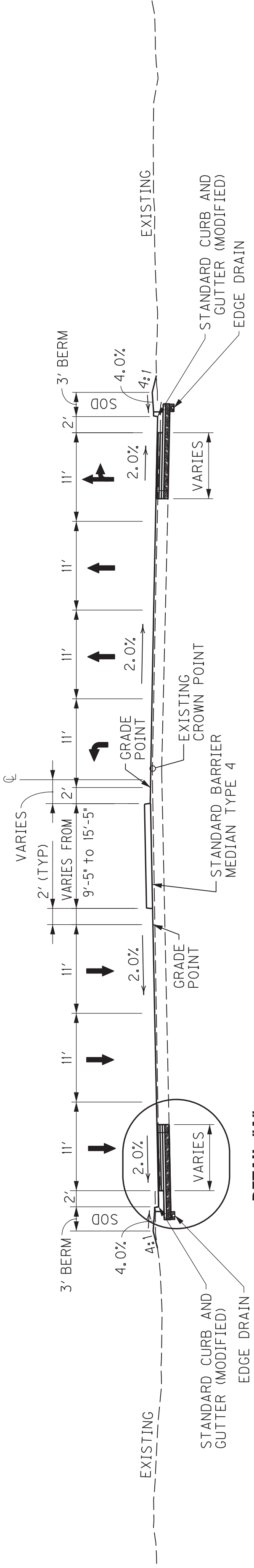
SCALE: NTS

TYPICAL SECTIONS

# TYPICAL SECTIONS US 60

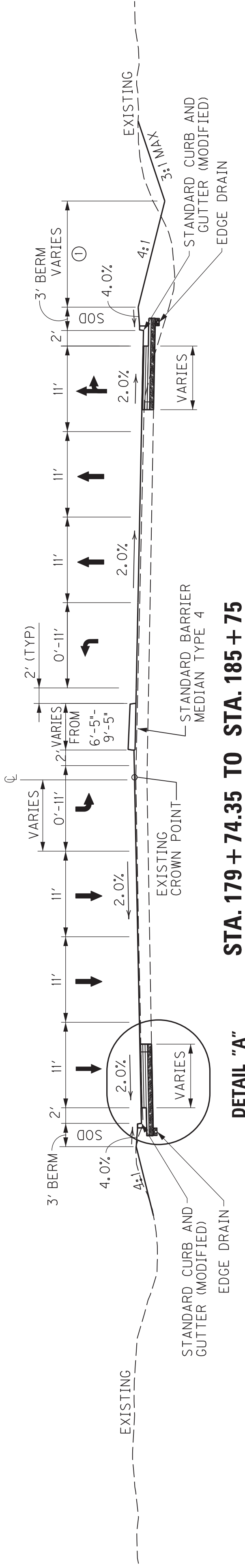
COUNTY OF	ITEM NO.	SHEET NO.
McCRACKEN	I-154.00	R2A

REVISED 11-30-18



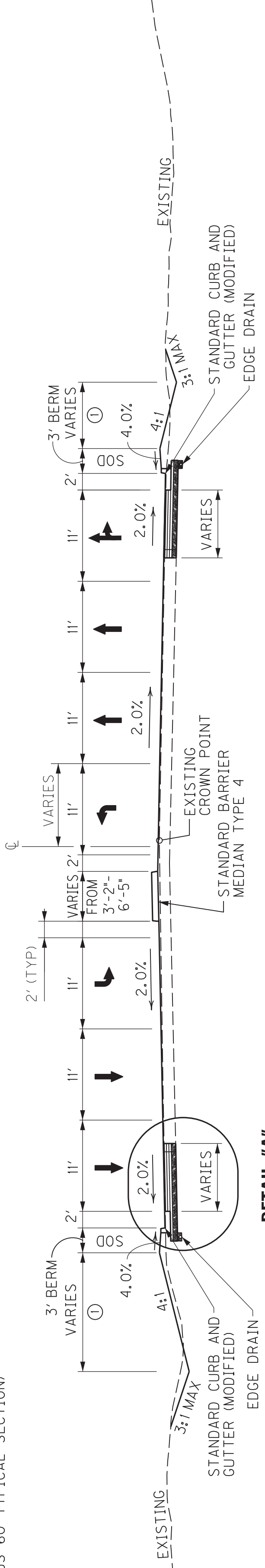
**STA. 185 + 75 TO STA. 188 + 98.31**

**DETAIL "A"**



**STA. 179 + 74.35 TO STA. 185 + 75**

**DETAIL "A"**



**STA. 176 + 25 TO STA. 179 + 74.35**

**DETAIL "A"**

NEW CONSTRUCTION  
GRADE, DRAIN & SURFACING  
- USING -

US 60 PAVEMENT

- 10.00" CRUSHED STONE BASE
- 4.00" CL3 ASPH. BASE 1,000 PG64-22
- 3.75" CL3 ASPH. BASE 1,000 PG64-22
- 3.75" CL3 ASPH. BASE 1,000 PG64-22

1.50" SURFACE  
1.50" CL3 ASPH. SURF. 0.50B PG76-22  
SHOULDERS

VARIES: STANDARD CURB AND GUTTER (MODIFIED)  
BARRIER MEDIAN TYPE 4 (SEE US 60 TYPICAL SECTION)

PAVEMENT OVERLAY  
- USING -  
US 60 PAVEMENT

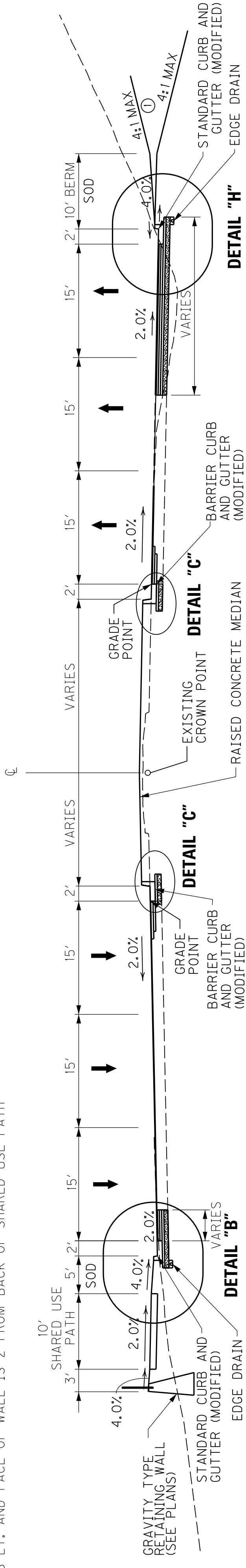
1.50" CL3 ASPH. SURF. 0.50B PG76-22  
WEDGES

① SEE CROSS SECTIONS FOR DITCH OFFSETS AND ELEVATIONS.

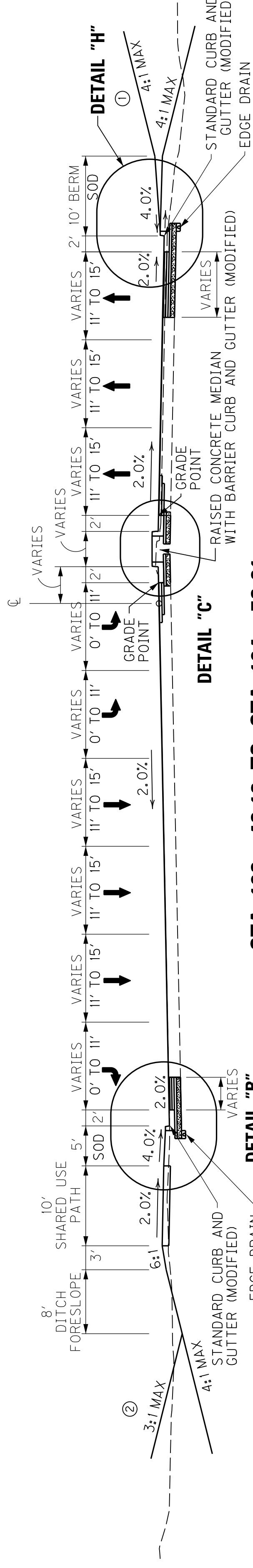
# TYPICAL SECTIONS US 60

COUNTY OF	ITEM NO.	SHEET NO.
McCRACKEN	I-154.00	R2B

NOTE: GRAVITY TYPE RETAINING WALL IS PRESENT FROM STA. 195+00.00 L.T. TO STA. 195+70.38 L.T. AND FACE OF WALL IS 2' FROM BACK OF SHARED USE PATH



**STA. 194 + 73.61 TO STA. 195 + 66.64**  
**STA. 204 + 82.87 TO STA. 205 + 89.36**



**STA. 193 + 43.12 TO STA. 194 + 73.61**

**NEW CONSTRUCTION  
GRADE, DRAIN & SURFACING**

**US 60 PAVEMENT**

- 21-50" BASE — [ 10.00" CRUSHED STONE BASE  
4.00" CL3 ASPH. BASE 1,000 PG64-22  
3.75" CL3 ASPH. BASE 1,000 PG64-22  
3.25" CL3 ASPH. BASE 1,000 PG64-22  
0.50" ASPHALT SCRATCH COURSE PG64-22 ]

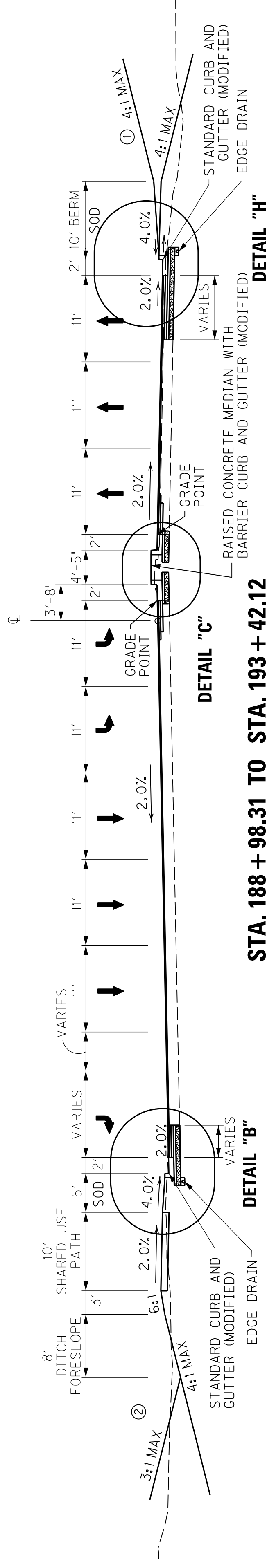
1.50" SURFACE — [ 1.50" CL3 ASPH. SURF. 0.50B PG76-22 ]

SHOULDERS  
 VARIES: STANDARD CURB AND GUTTER (MODIFIED)  
 BARRIER CURB AND GUTTER (MODIFIED) (SEE US 60 TYPICAL SECTION)

**PAVEMENT OVERLAY**  
 - USING -

**US 60 PAVEMENT**

LEVEL & WEDGE PG64-22  
 1.50" CL3 ASPH. SURF. 0.50B PG76-22



**STA. 188 + 98.31 TO STA. 193 + 42.12**

NO RAISED MEDIAN FROM STA. 188+98.31 TO STA. 190+51.81

- ① SEE CROSS SECTIONS FOR CONSTRUCTION OUTSIDE THE SHARED USE PATH OR BERM.
- ② SEE CROSS SECTIONS FOR DITCH OFFSETS AND ELEVATIONS.

FILE NAME: V:\1785\ACTIVE\178565008\TRANSPORTATION\DESIGN\DRAWING\PLAN\SHEETS\TYPICALS.DWG  
 DATE PLOTTED: November 30, 2018  
 USER: kdeep

E-SHEET NAME: MicroStation v8.11.9.832

SCALE: NTS

TYPICAL SECTIONS

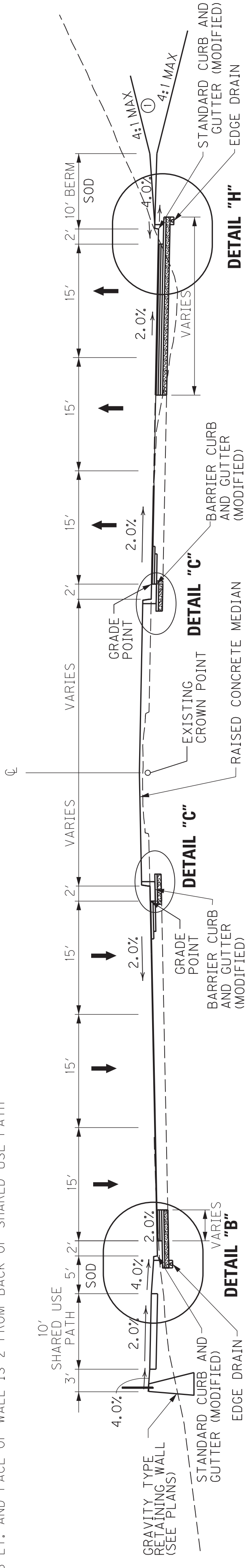


# TYPICAL SECTIONS US 60

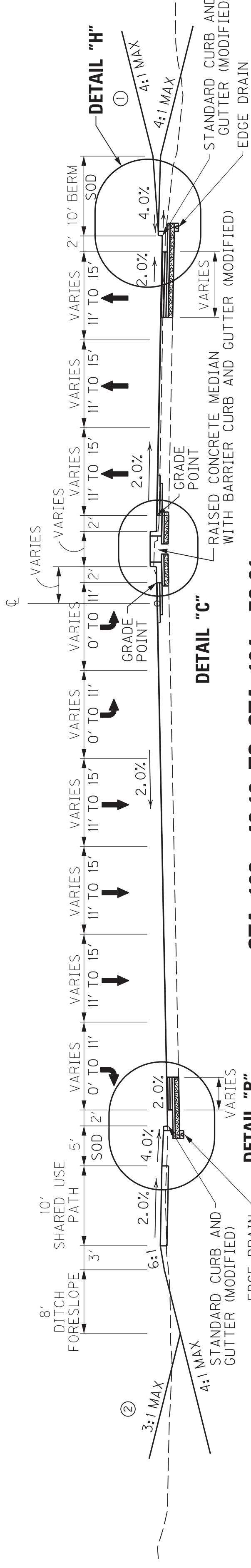
COUNTY OF	ITEM NO.	SHEET NO.
McCRACKEN	I-154.00	R2B

REVISED 11-30-18

NOTE: GRAVITY TYPE RETAINING WALL IS PRESENT FROM STA. 195+00.00 L.T. TO STA. 195+70.38 L.T. AND FACE OF WALL IS 2' FROM BACK OF SHARED USE PATH



**STA. 194 + 73.61 TO STA. 195 + 66.64**  
**STA. 204 + 82.87 TO STA. 205 + 89.36**



**STA. 193 + 43.12 TO STA. 194 + 73.61**

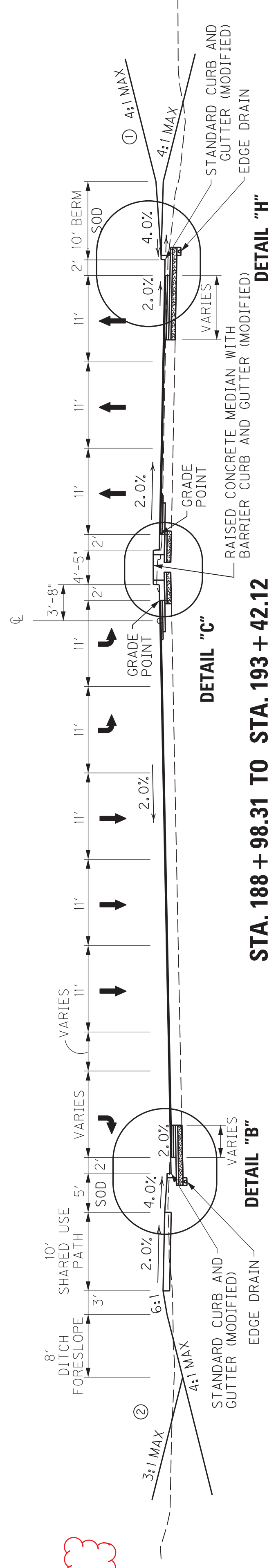
**NEW CONSTRUCTION  
GRADE, DRAIN & SURFACING**

- 21.50" BASE
- 10.00' CRUSHED STONE BASE
- 4.00" CL3 ASPH. BASE 1,000 PG64-22
- 3.75" CL3 ASPH. BASE 1,000 PG64-22
- 3.25" CL3 ASPH. BASE 1,000 PG64-22
- 0.50" ASPHALT SCRATCH COURSE PG64-22
- 1.50" SURFACE
- 1.50" CL3 ASPH. SURF. 0.50B PG76-22

**SHOULDERS**  
 VARIES: STANDARD CURB AND GUTTER (MODIFIED)  
 BARRIER CURB AND GUTTER (MODIFIED) (SEE US 60 TYPICAL SECTION)

**PAVEMENT OVERLAY**  
 - USING -  
**US 60 PAVEMENT**

- 10.00' CRUSHED STONE BASE
- 4.00" CL3 ASPH. SURF. 0.50B PG76-22
- 1.50" CL3 ASPH. SURF. 0.50B PG76-22



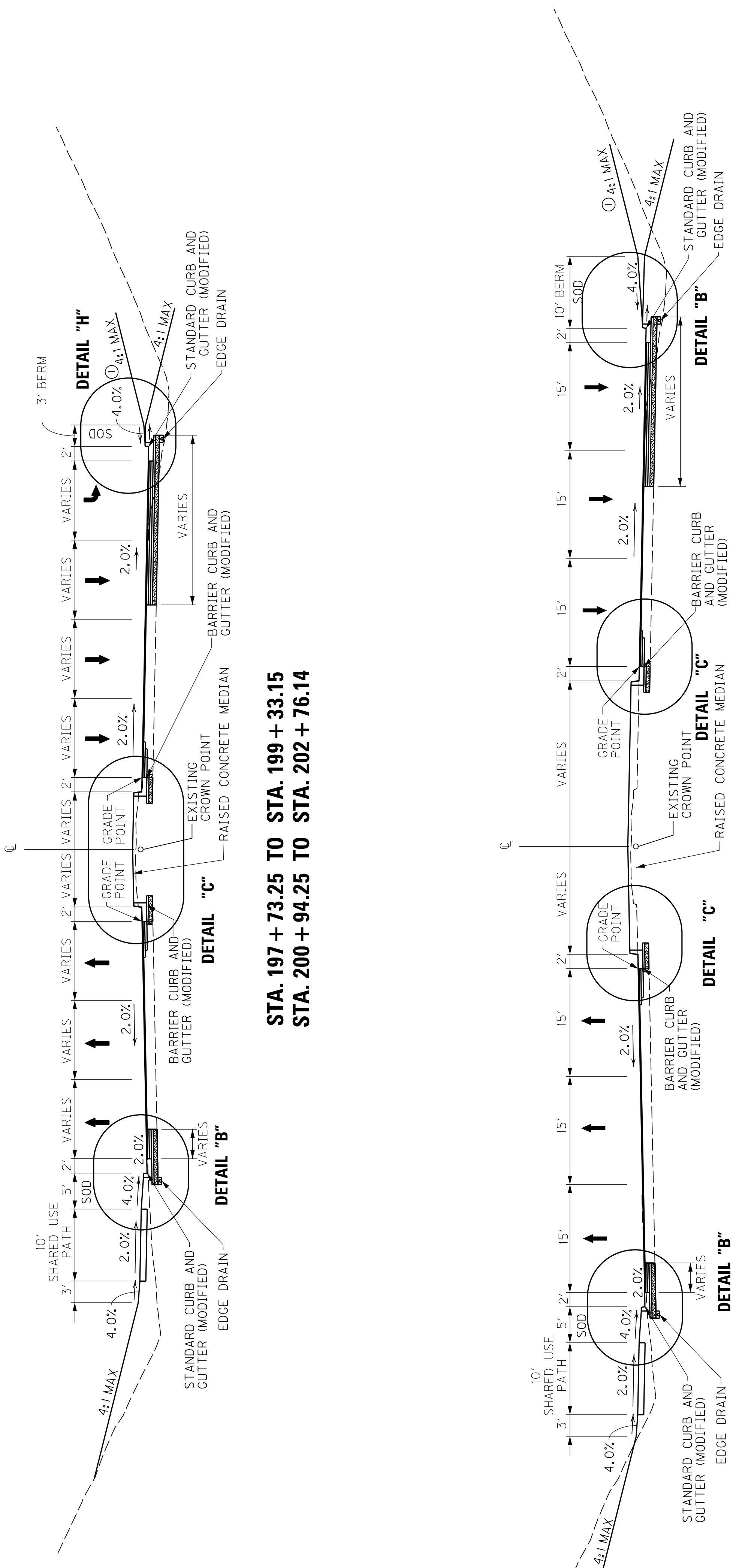
**STA. 188 + 98.31 TO STA. 193 + 42.12**

NO RAISED MEDIAN FROM STA. 188+98.31 TO STA. 190+51.81

- ① SEE CROSS SECTIONS FOR CONSTRUCTION OUTSIDE THE SHARED USE PATH OR BERM.
- ② SEE CROSS SECTIONS FOR DITCH OFFSETS AND ELEVATIONS.

# TYPICAL SECTIONS US 60

COUNTY OF	ITEM NO.	SHEET NO.
McCRACKEN	1-154.00	R2C



**STA. 197 + 73.25 TO STA. 199 + 33.15**  
**STA. 200 + 94.25 TO STA. 202 + 76.14**

**STA. 196 + 73.12 TO STA. 197 + 73.25**  
**STA. 202 + 76.14 TO STA. 203 + 76.39**

**NEW CONSTRUCTION  
GRADE, DRAIN & SURFACING**  
 - USING -

**US 60 PAVEMENT**

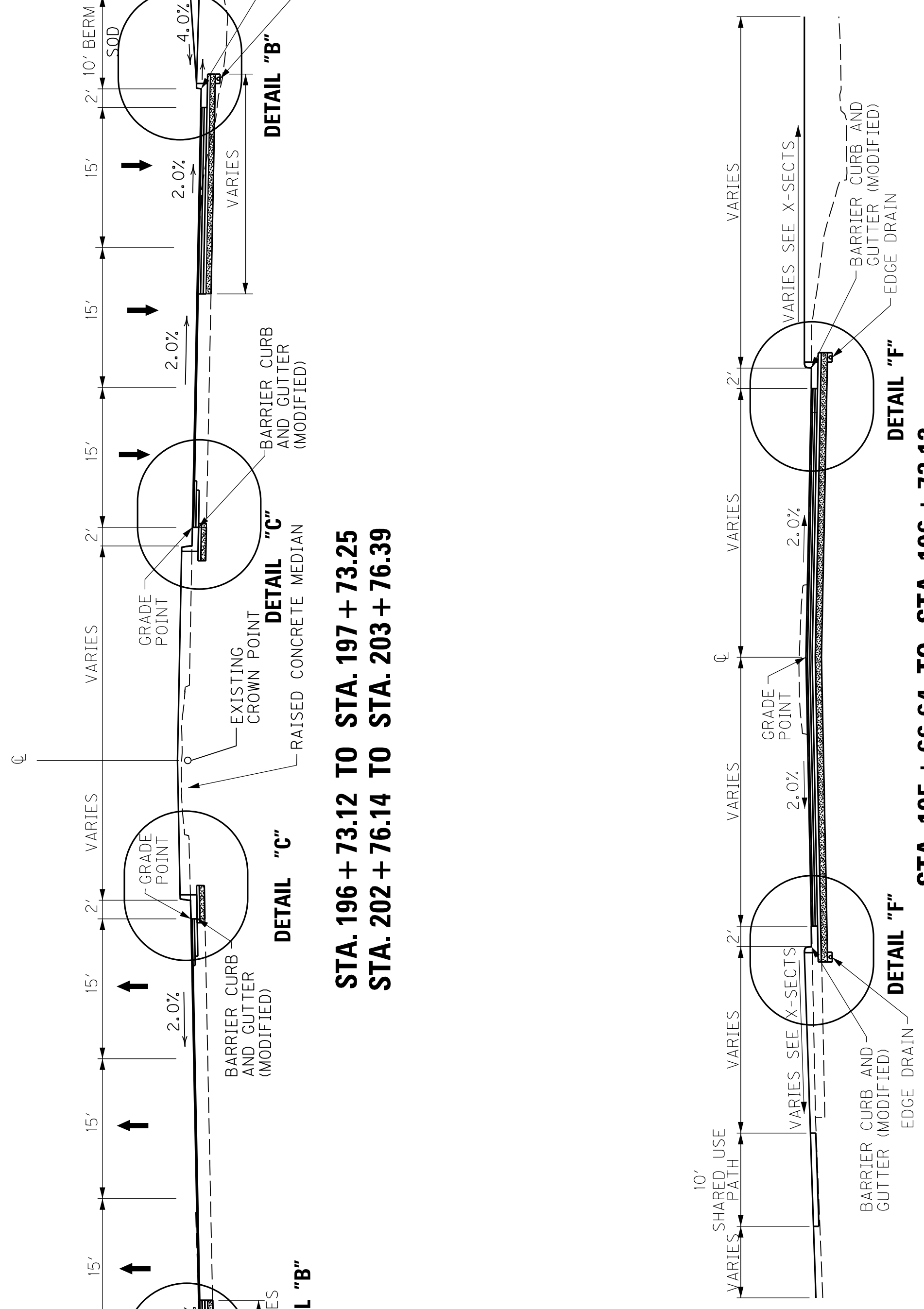
- 21.50' BASE — [ 10.00' CRUSHED STONE BASE
- 4.00' CL3 ASPH. BASE 1,000 PG64-22
- 3.75' CL3 ASPH. BASE 1,000 PG64-22
- 3.25' CL3 ASPH. BASE 1,000 PG64-22
- 0.50' ASPHALT SCRATCH COURSE PG64-22
- 1.50' SURFACE — [ 1.50' CL3 ASPH. SURF. 0.50B PG76-22

**SHOULDERS**  
 VARIES; STANDARD CURB AND GUTTER (MODIFIED)  
 BARRIER CURB AND GUTTER (MODIFIED) (SEE US 60 TYPICAL SECTION)

**PAVEMENT OVERLAY**  
 - USING -

**US 60 PAVEMENT**

- LEVEL & WEDGE PG64-22
- 1.50' CL3 ASPH. SURF. 0.50B PG76-22



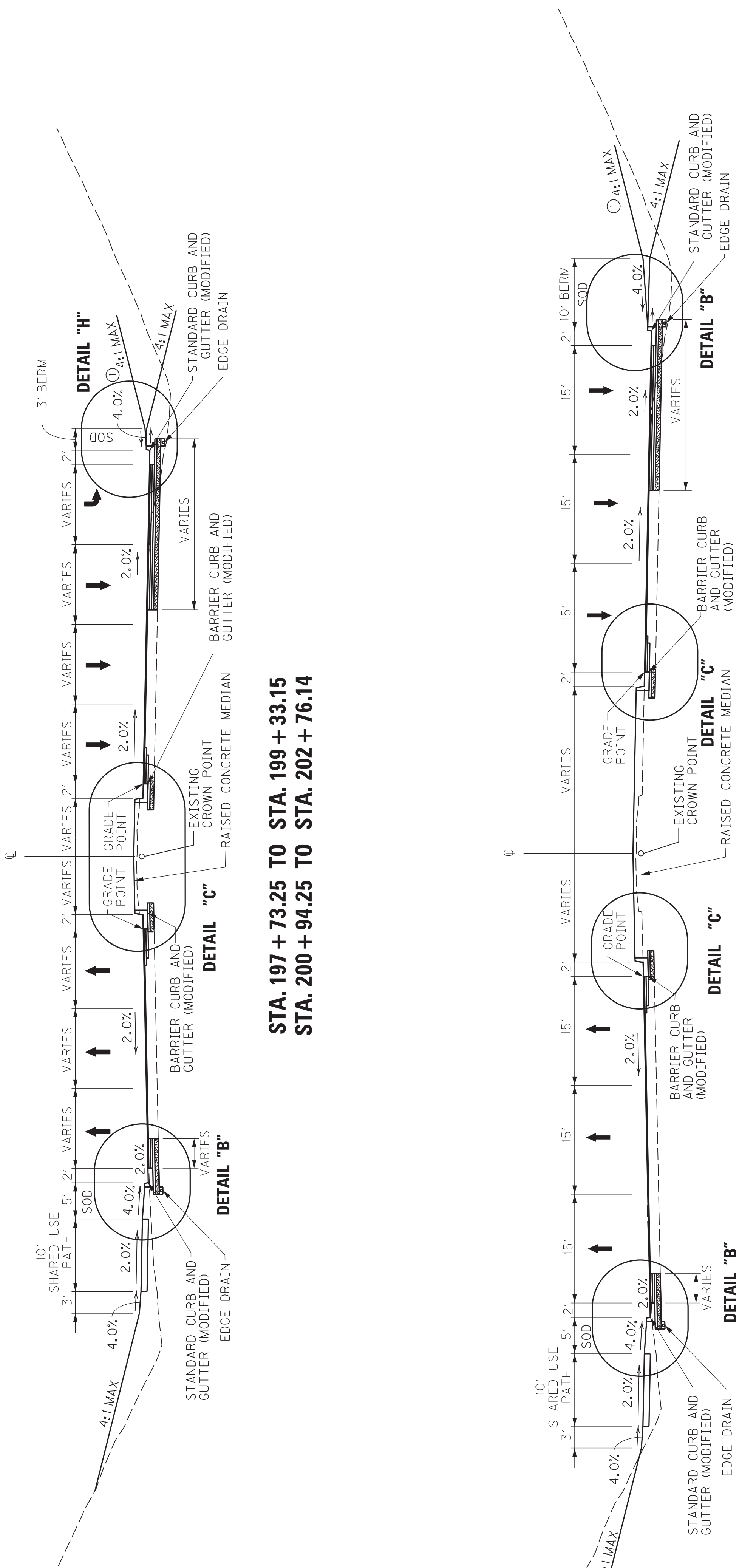
**STA. 195 + 66.64 TO STA. 196 + 73.12**  
**STA. 203 + 76.39 TO STA. 204 + 82.87**

① SEE CROSS SECTIONS FOR CONSTRUCTION OUTSIDE THE SHARED USE PATH OR BERM.

# TYPICAL SECTIONS US 60

COUNTY OF	ITEM NO.	SHEET NO.
McCRACKEN	I-154.00	R2C

REVISED 11-30-18



STA. 197 + 73.25 TO STA. 199 + 33.15  
 STA. 200 + 94.25 TO STA. 202 + 76.14

STA. 196 + 73.12 TO STA. 197 + 73.25  
 STA. 202 + 76.14 TO STA. 203 + 76.39

NEW CONSTRUCTION  
 GRADE, DRAIN & SURFACING  
 - USING -

**US 60 PAVEMENT**

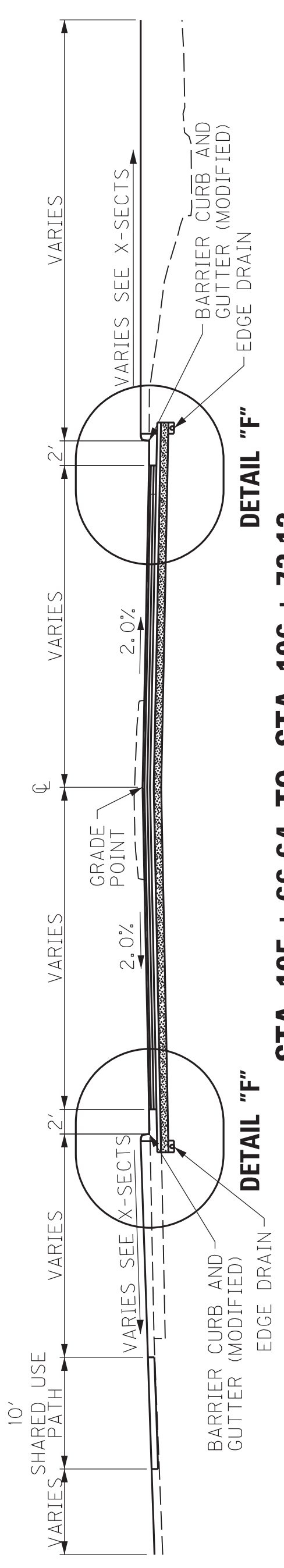
- 10.00' CRUSHED STONE BASE
- 4.00' CL3 ASPH. BASE 1,000 PG64-22
- 3.75' CL3 ASPH. BASE 1,000 PG64-22
- 3.25' CL3 ASPH. BASE 1,000 PG64-22
- 0.50' ASPHALT SCRATCH COURSE PG64-22
- 1.50' SURFACE 1.50' CL3 ASPH. SURF. 0.50B PG76-22

SHOULDERS  
 VARIES; STANDARD CURB AND GUTTER (MODIFIED)  
 BARRIER CURB AND GUTTER (MODIFIED) (SEE US 60 TYPICAL SECTION)

**PAVEMENT OVERLAY**  
 - USING -

**US 60 PAVEMENT**

- 1.50' CL3 ASPH. SURF. 0.50B PG76-22

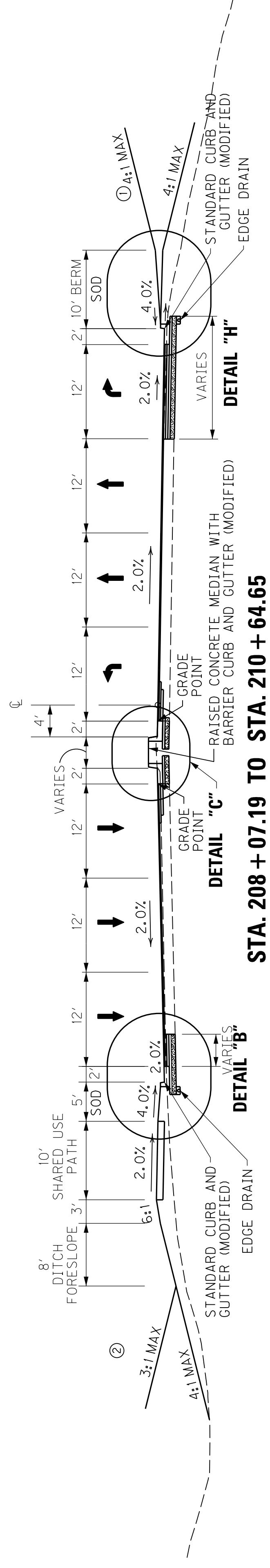


STA. 195 + 66.64 TO STA. 196 + 73.12  
 STA. 203 + 76.39 TO STA. 204 + 82.87

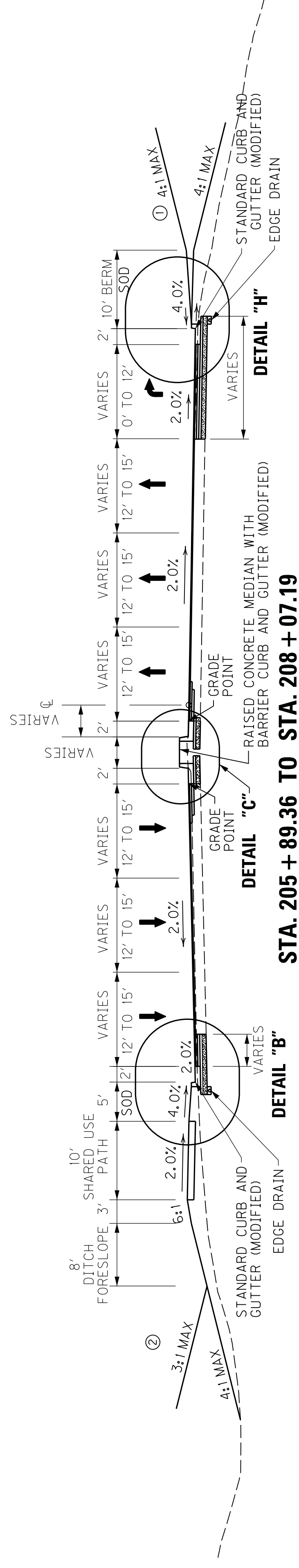
SEE CROSS SECTIONS FOR CONSTRUCTION OUTSIDE THE SHARED USE PATH OR BERM.

# TYPICAL SECTIONS US 60

COUNTY OF	ITEM NO.	SHEET NO.
McCRACKEN	I-154.00	R2D



STA. 208 + 07.19 TO STA. 210 + 64.65



STA. 205 + 89.36 TO STA. 208 + 07.19

**NEW CONSTRUCTION  
GRADE, DRAIN & SURFACING  
- USING -**

**US 60 PAVEMENT**

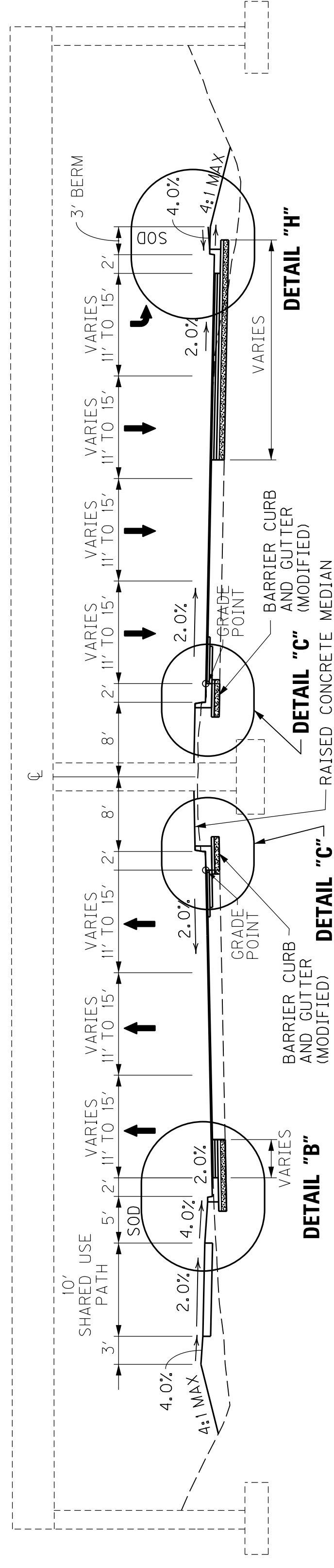
- 21.50' BASE — [10.00' CRUSHED STONE BASE  
4.00' CL3 ASPH. BASE 1,000 PG64-22  
3.75' CL3 ASPH. BASE 1,000 PG64-22  
3.25' CL3 ASPH. BASE 1,000 PG64-22  
0.50' ASPHALT SCRATCH COURSE PG64-22
- 1.50' SURFACE — [1.50' CL3 ASPH. SURF. 0.50B PG76-22

**SHOULDERS**  
VARIES; STANDARD CURB AND GUTTER (MODIFIED)  
BARRIER CURB AND GUTTER (MODIFIED) (SEE US 60 TYPICAL SECTION)

**PAVEMENT OVERLAY  
- USING -**

**US 60 PAVEMENT**

- LEVEL & WEDGE PG64-22
- 1.50' CL3 ASPH. SURF. 0.50B PG76-22



STA. 199 + 33.15 TO STA. 200 + 94.25  
UNDER EXISTING INTERSTATE BRIDGE

- ① SEE CROSS SECTIONS FOR CONSTRUCTION OUTSIDE THE SHARED USE PATH OR BERM.
- ② SEE CROSS SECTIONS FOR DITCH OFFSETS AND ELEVATIONS.

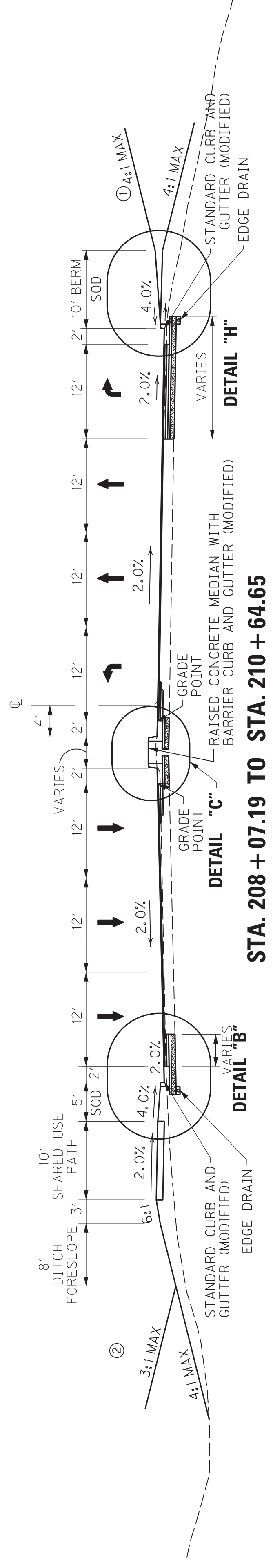
SCALE: NTS

TYPICAL SECTIONS

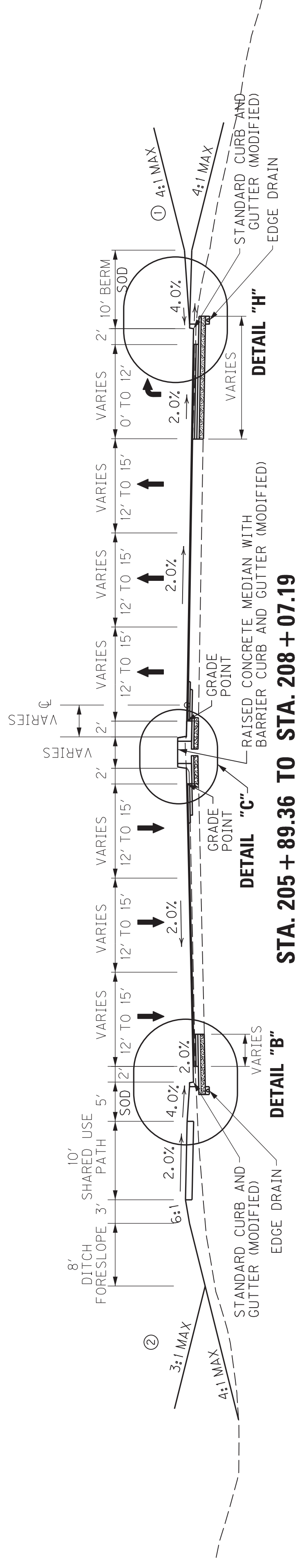
# TYPICAL SECTIONS US 60

COUNTY OF	ITEM NO.	SHEET NO.
McCRACKEN	I-154.00	R2D

REVISED 11-30-18



STA. 208 + 07.19 TO STA. 210 + 64.65



STA. 205 + 89.36 TO STA. 208 + 07.19

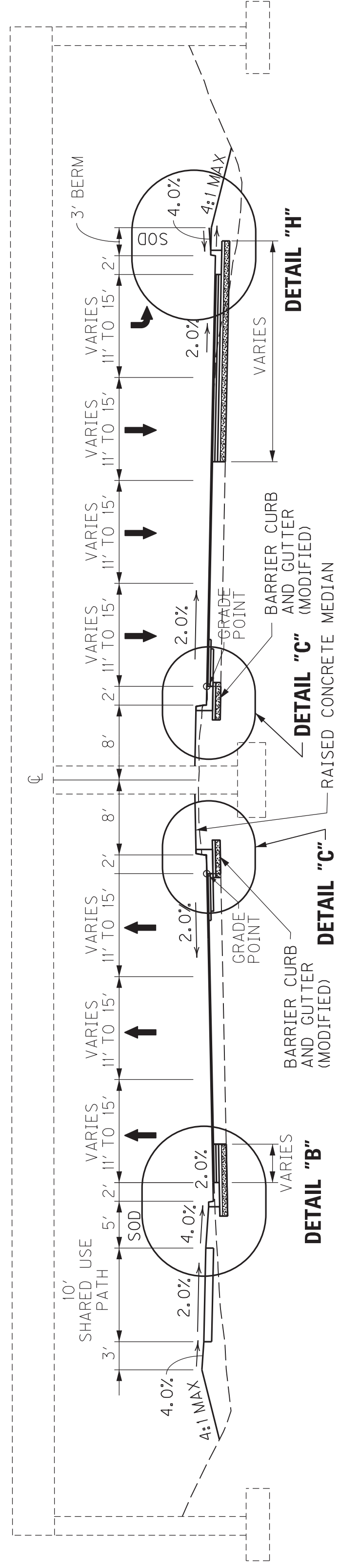
NEW CONSTRUCTION  
GRADE, DRAIN & SURFACING  
- USING -

- US 60 PAVEMENT
- 10.00" CRUSHED STONE BASE
- 4.00" CL3 ASPH. BASE 1,000 PG64-22
- 3.75" CL3 ASPH. BASE 1,000 PG64-22
- 3.25" CL3 ASPH. BASE 1,000 PG64-22
- 0.50" ASPHALT SCRATCH COURSE PG64-22
- SHOULDERS
- 1.50" CL3 ASPH. SURF. 0.50B PG76-22

VARIES; STANDARD CURB AND GUTTER (MODIFIED)  
BARRIER CURB AND GUTTER (MODIFIED) (SEE US 60 TYPICAL SECTION)

PAVEMENT OVERLAY  
- USING -

- US 60 PAVEMENT
- 1.50" CL3 ASPH. SURF. 0.50B PG76-22



STA. 199 + 33.15 TO STA. 200 + 94.25  
UNDER EXISTING INTERSTATE BRIDGE

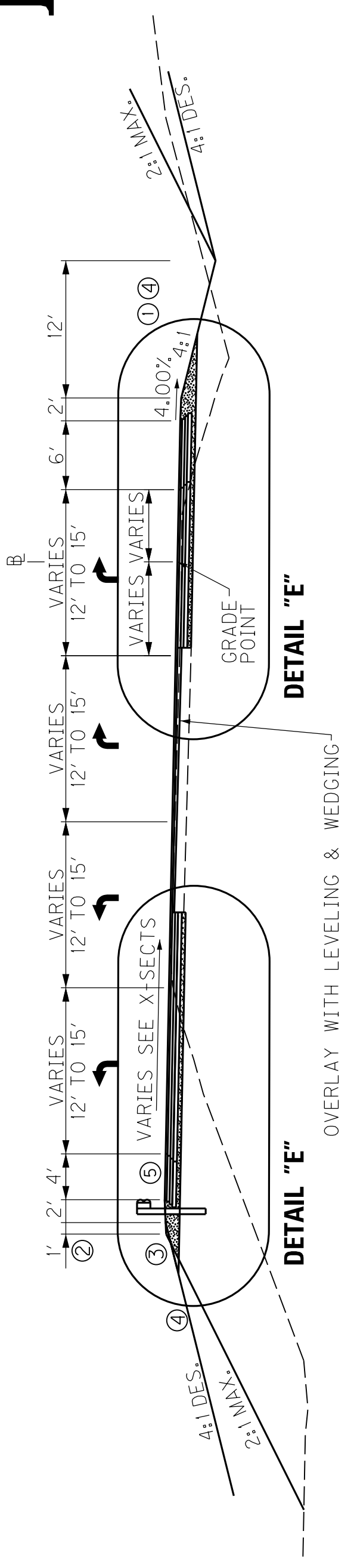
- ① SEE CROSS SECTIONS FOR CONSTRUCTION OUTSIDE THE SHARED USE PATH OR BERM.
- ② SEE CROSS SECTIONS FOR DITCH OFFSETS AND ELEVATIONS.

SCALE: NTS

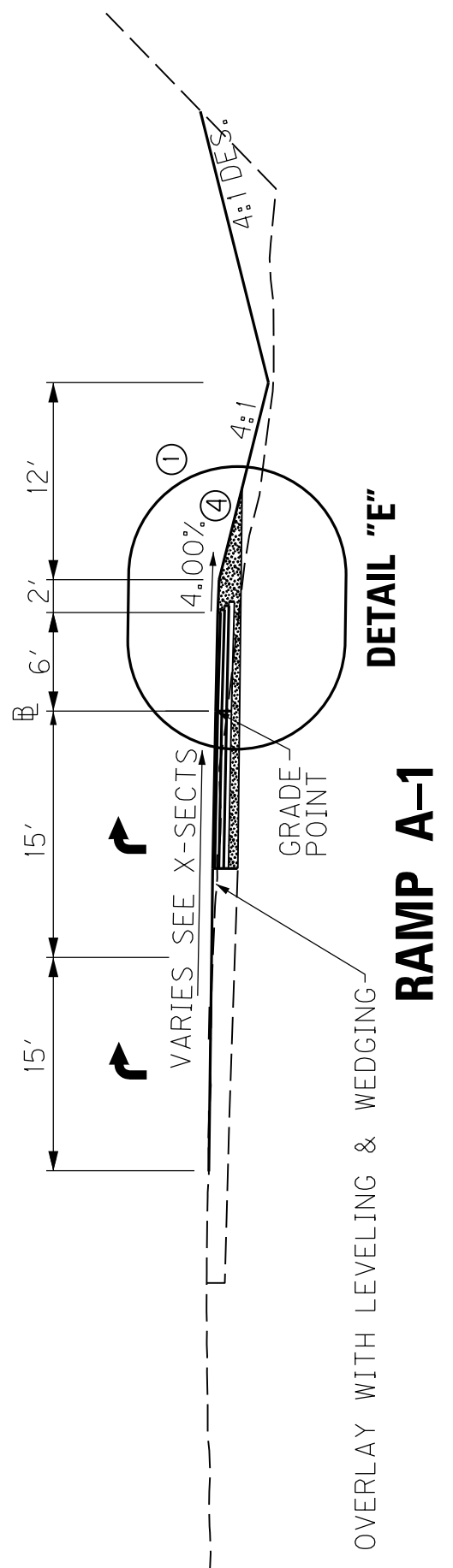
TYPICAL SECTIONS

# TYPICAL SECTIONS RAMPS

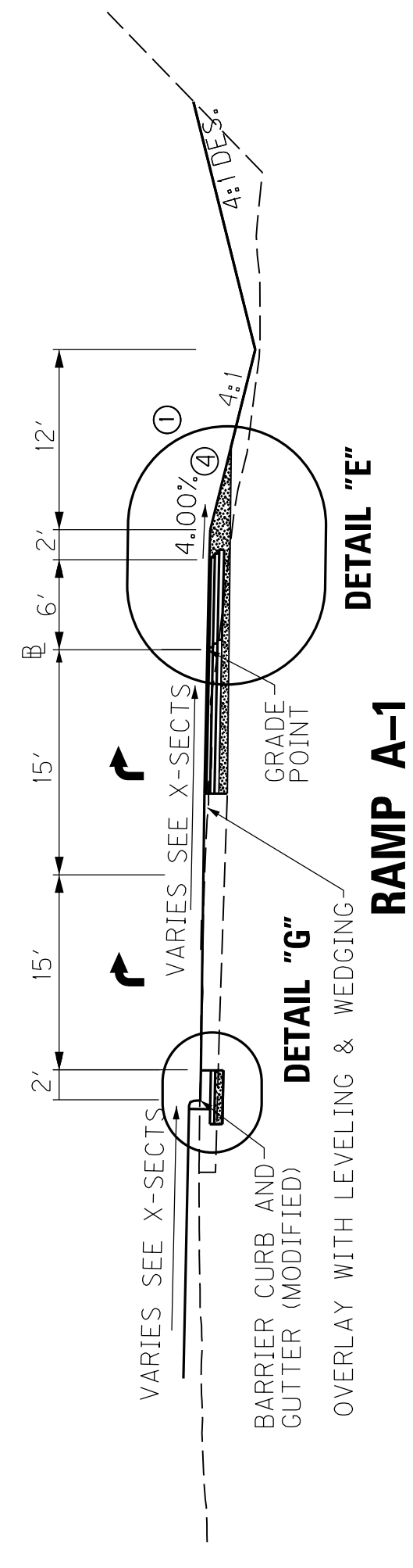
COUNTY OF	ITEM NO.	SHEET NO.
McCRACKEN	I-154-00	R2F



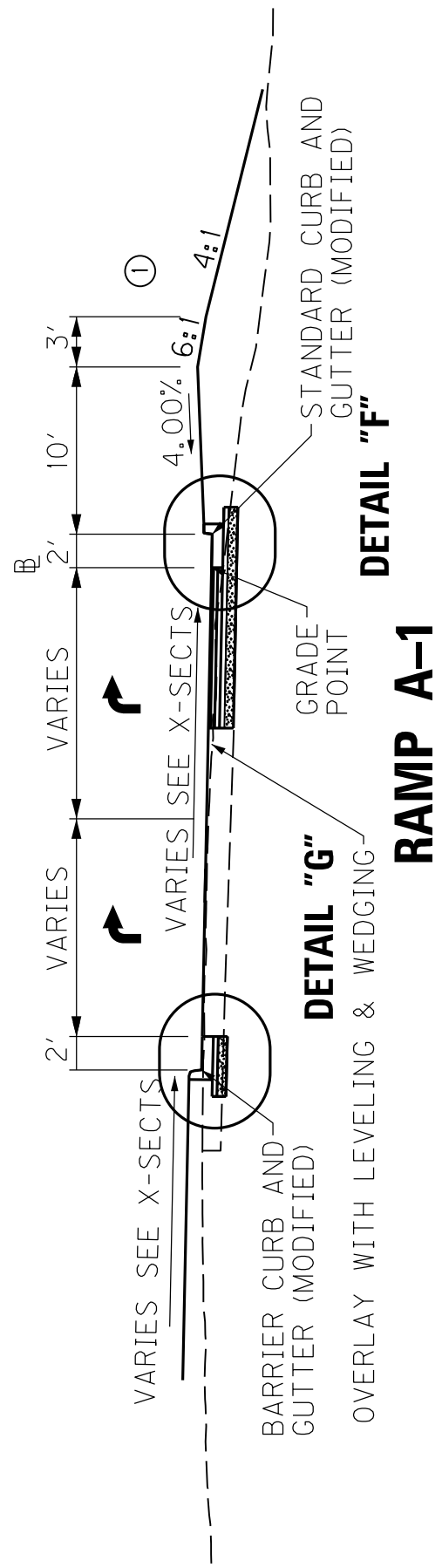
**RAMP A**  
**STA. 5 + 38.99 TO STA. 109 + 67.04**  
NOTE: STA. 9+13.99 BACK = STA. 109+13.99 AHEAD



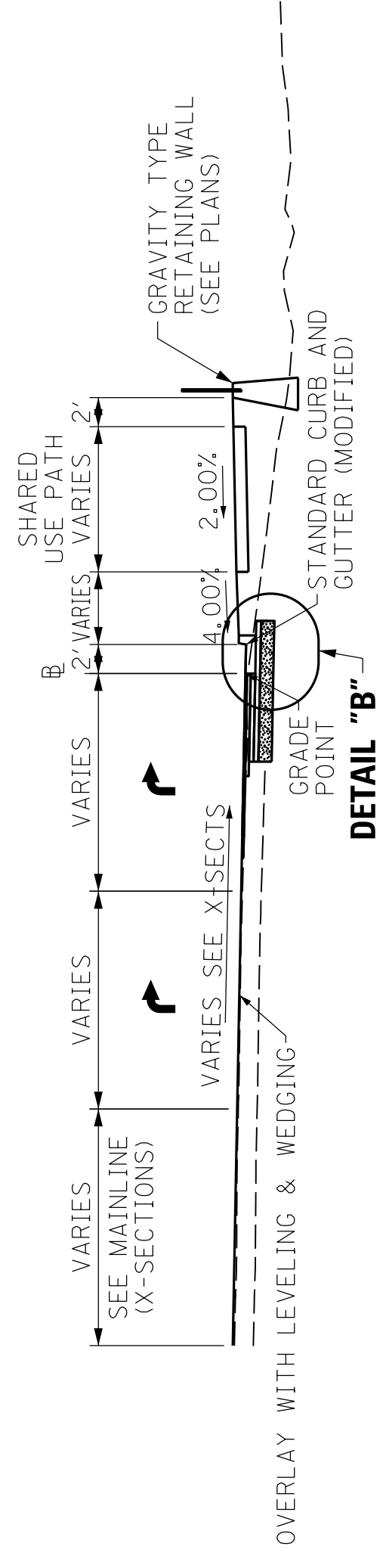
**RAMP A-1**  
**STA. 109 + 67.04 TO STA. 110 + 17.02**



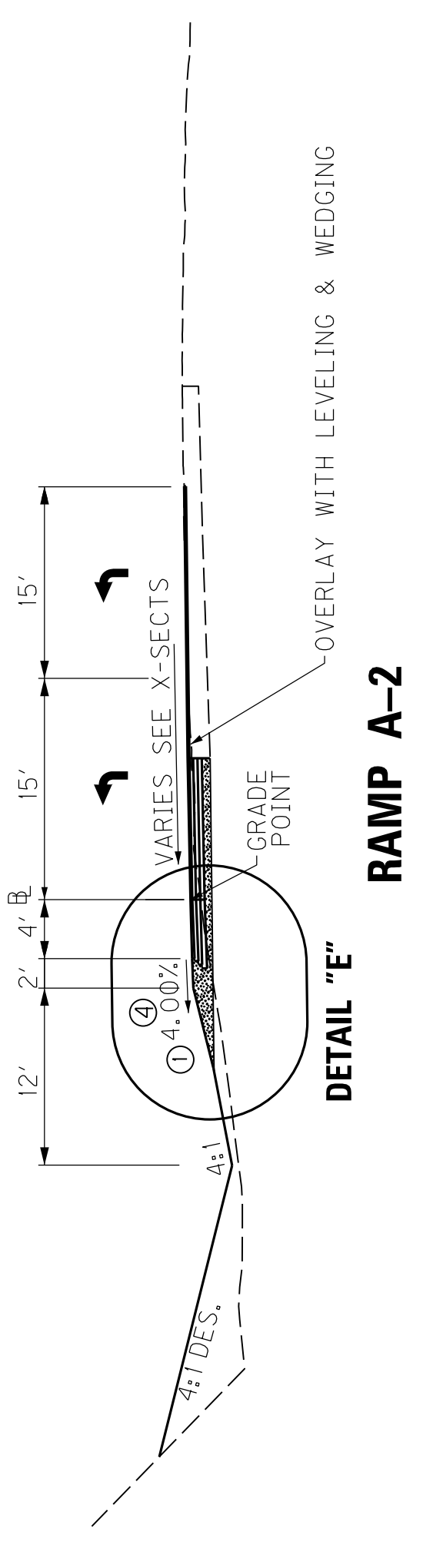
**RAMP A-1**  
**STA. 110 + 17.02 TO STA. 111 + 11.00**



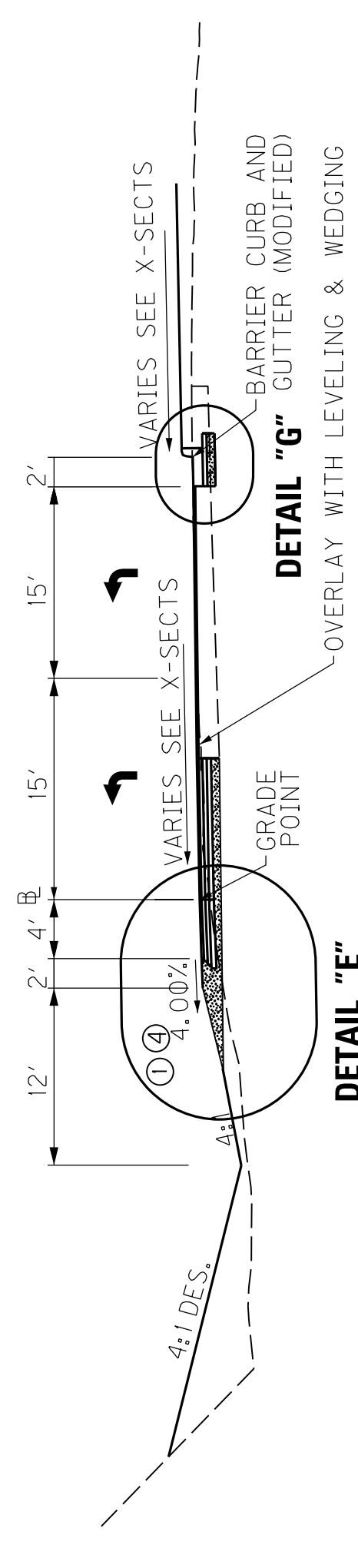
**RAMP A-1**  
**STA. 111 + 11.00 TO STA. 111 + 80.00**



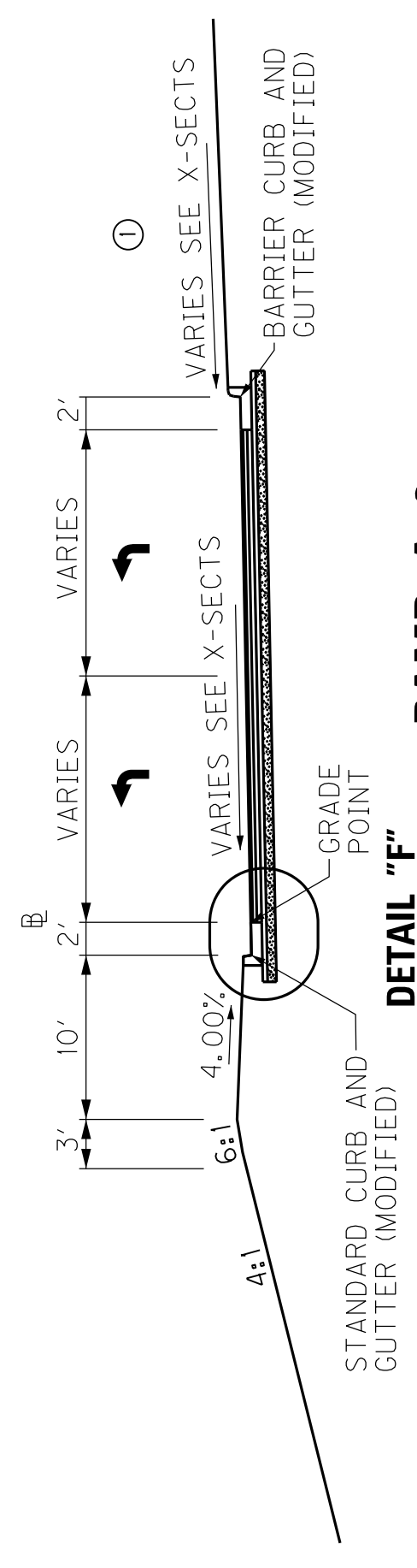
**RAMP A-1**  
**STA. 111 + 80.00 TO STA. 112 + 40.71**



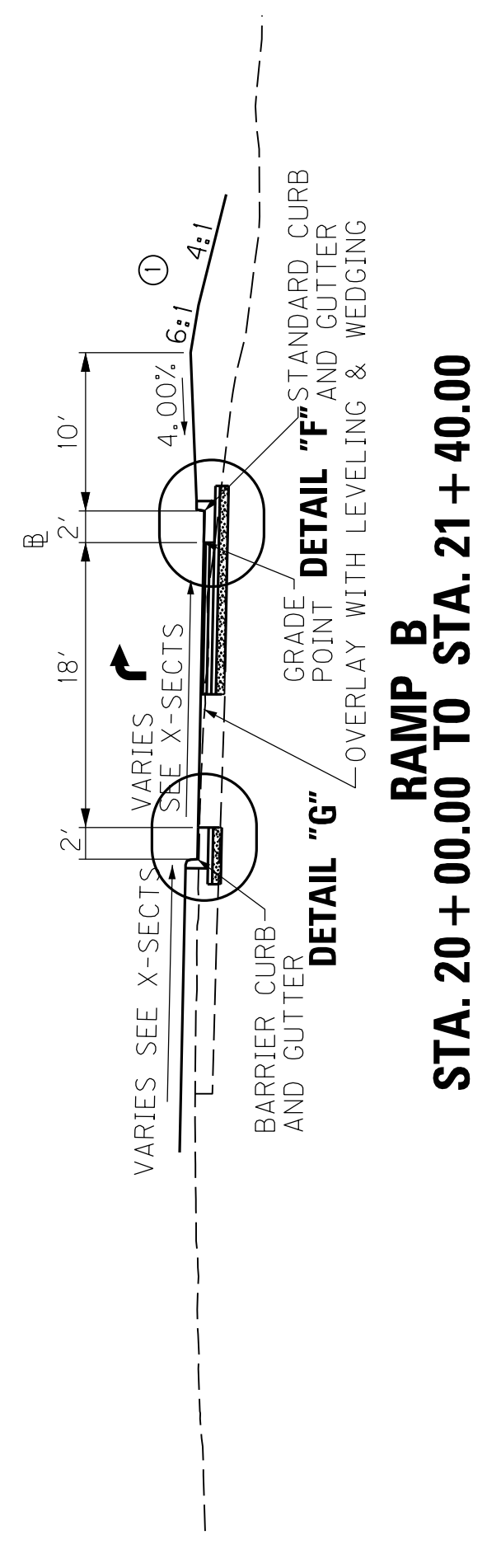
**RAMP A-2**  
**STA. 209 + 67.04 TO STA. 210 + 05.03**



**RAMP A-2**  
**STA. 210 + 05.03 TO STA. 210 + 15.60**



**RAMP A-2**  
**STA. 210 + 15.60 TO 211 + 60.61**



**RAMP B**  
**STA. 20 + 00.00 TO STA. 21 + 40.00**

- NEW CONSTRUCTION GRADE, DRAJG & SURFACING - USING -**
- RAMP PAVEMENT**
- 21-50" BASE
    - 10.00" CRUSHED STONE BASE
    - 4.00" CL3 ASPH. BASE 1,000 PG64-22
    - 3.75" CL3 ASPH. BASE 1,000 PG64-22
    - 3.25" CL3 ASPH. BASE 1,000 PG64-22
    - 0.50" ASPHALT SCRATCH COURSE PG64-22
  - 1.50" SURFACE
    - 1.50" CL3 ASPH. SURF. 0.50B PG76-22
- SHOULDERS:  
VARIES: STANDARD CURB AND GUTTER (MODIFIED)
- OR
- 1.50" SURFACE
    - 1.50" CL3 ASPH. SURF. 0.50B PG76-22
  - 21-50" BASE
    - 10.00" CRUSHED STONE BASE
    - 4.00" CL3 ASPH. BASE 1,000 PG64-22
    - 3.75" CL3 ASPH. BASE 1,000 PG64-22
    - 3.25" CL3 ASPH. BASE 1,000 PG64-22
    - 0.50" ASPHALT SCRATCH COURSE PG64-22

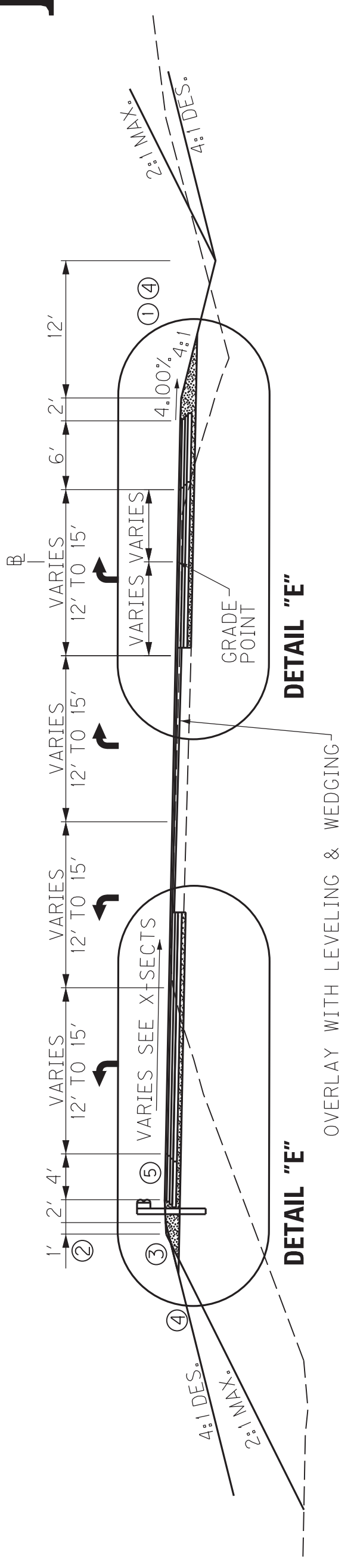
- PAVEMENT OVERLAY - USING -**
- RAMP PAVEMENT**
- LEVEL & WEDGE PG64-22
  - 1.50" CL3 ASPH. SURF. 0.50B PG76-22

- ① SEE CROSS SECTIONS FOR CONSTRUCTION OUTSIDE THE SHARED USE PATH OR BERM.
- ② SHOULDER SHALL BE WIDENED 1' WHERE GUARDRAIL IS REQUIRED
- ③ SUPERELEVATED SHOULDERS SHALL BE CONSTRUCTED TO STANDARD SUPERELEVATION, EXCEPT NOT FLATTER THAN THE SLOPES INDICATED FOR NORMAL SHOULDERS.
- ④ ASPHALT SEAL REQUIRED FROM OUTSIDE EDGE OF PAVED SHOULDER TO A POINT 2' DOWN THE DITCH OR FILL SLOPE. TWO APPLICATIONS OF THE FOLLOWING:  
ASPHALT SEAL COAT - 2.4 LB/SY  
ASPHALT SEAL AGGREGATE - 20 LB/SY (SIZE NO. 8 OR 9M)

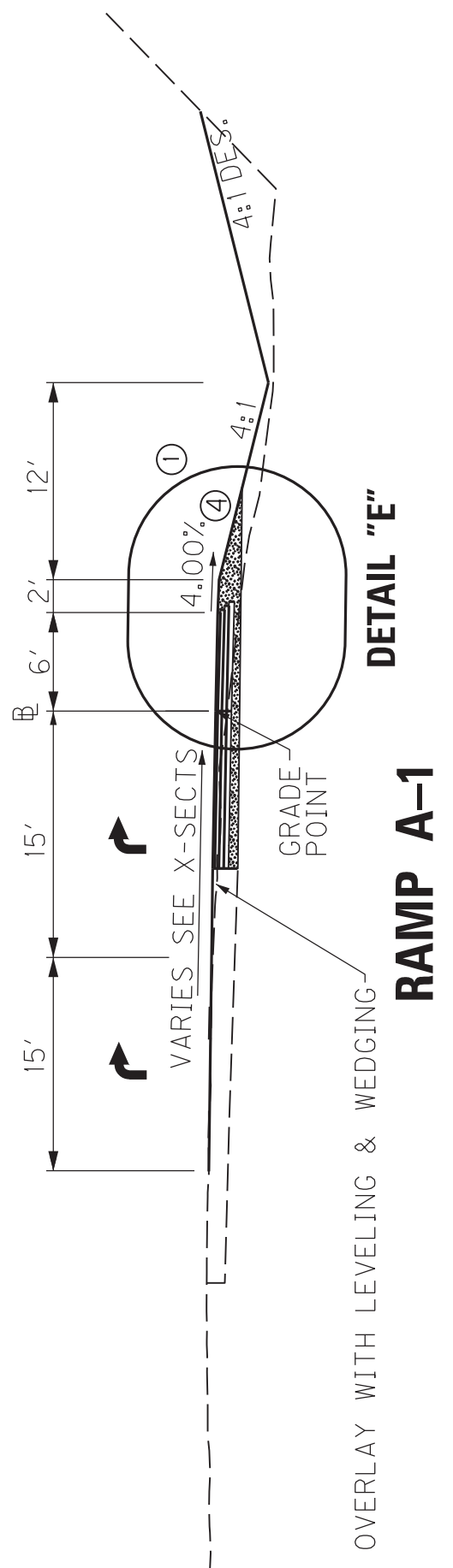
# TYPICAL SECTIONS RAMPS

COUNTY OF	ITEM NO.	SHEET NO.
McCRACKEN	I-154.00	R2F

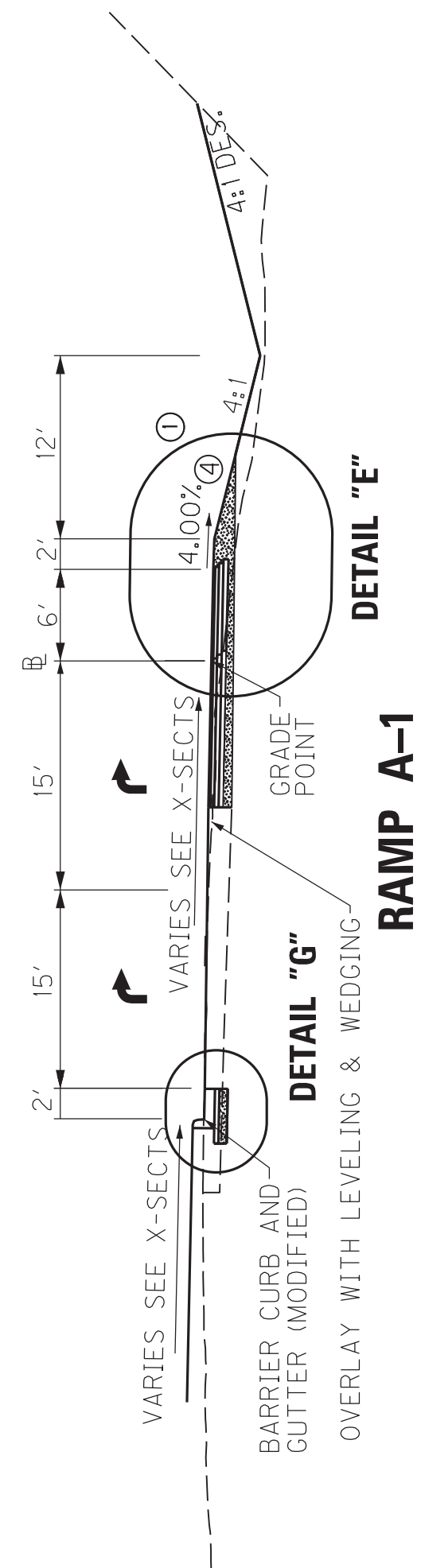
REVISED 11-30-18



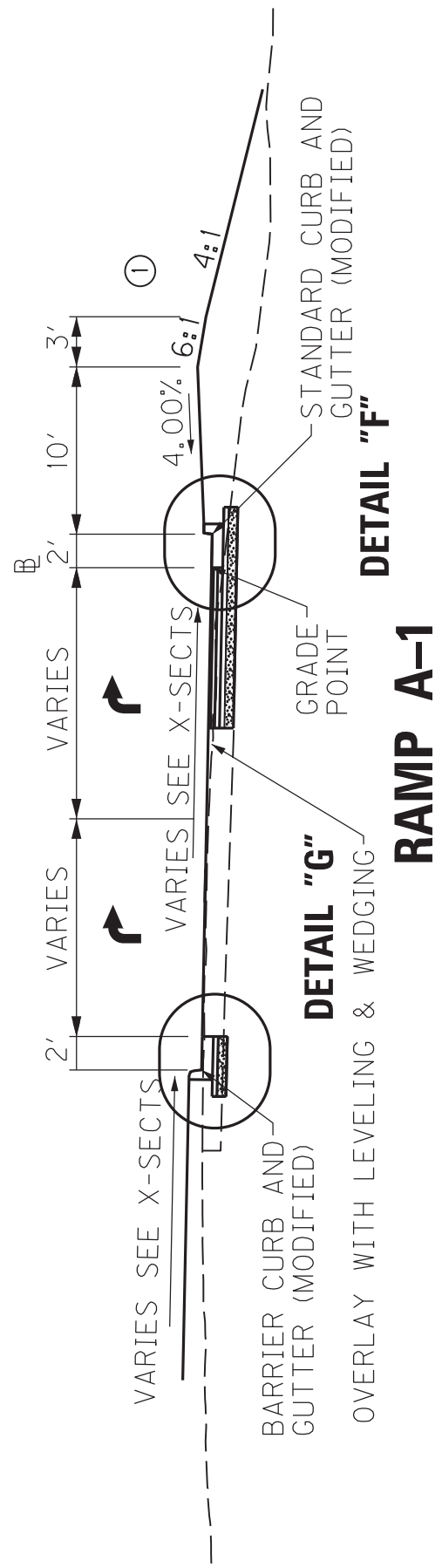
**RAMP A**  
**STA. 5 + 38.99 TO STA. 109 + 67.04**  
NOTE: STA. 9+13.99 BACK = STA. 109+13.99 AHEAD



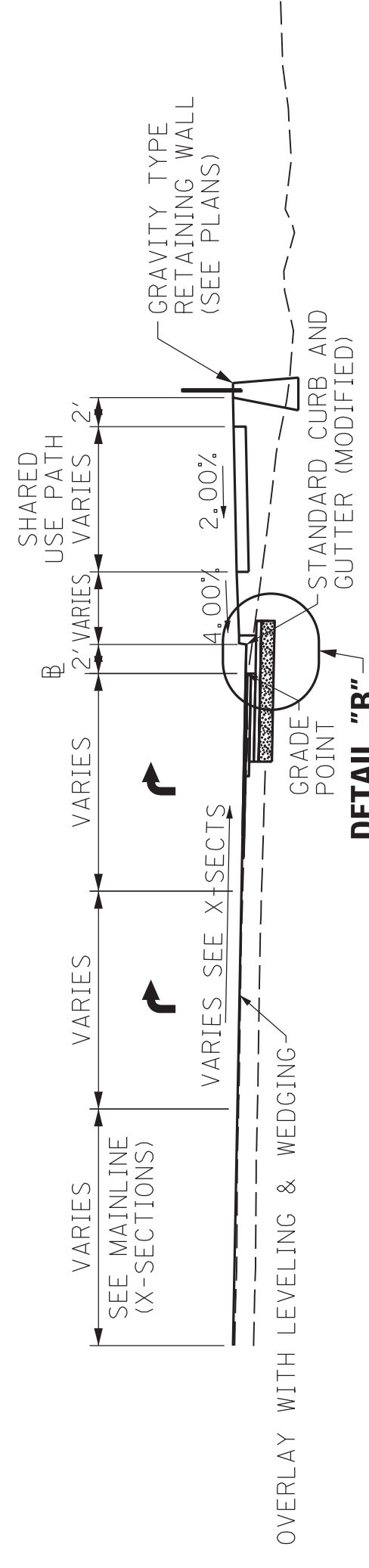
**RAMP A-1**  
**STA. 109 + 67.04 TO STA. 110 + 17.02**



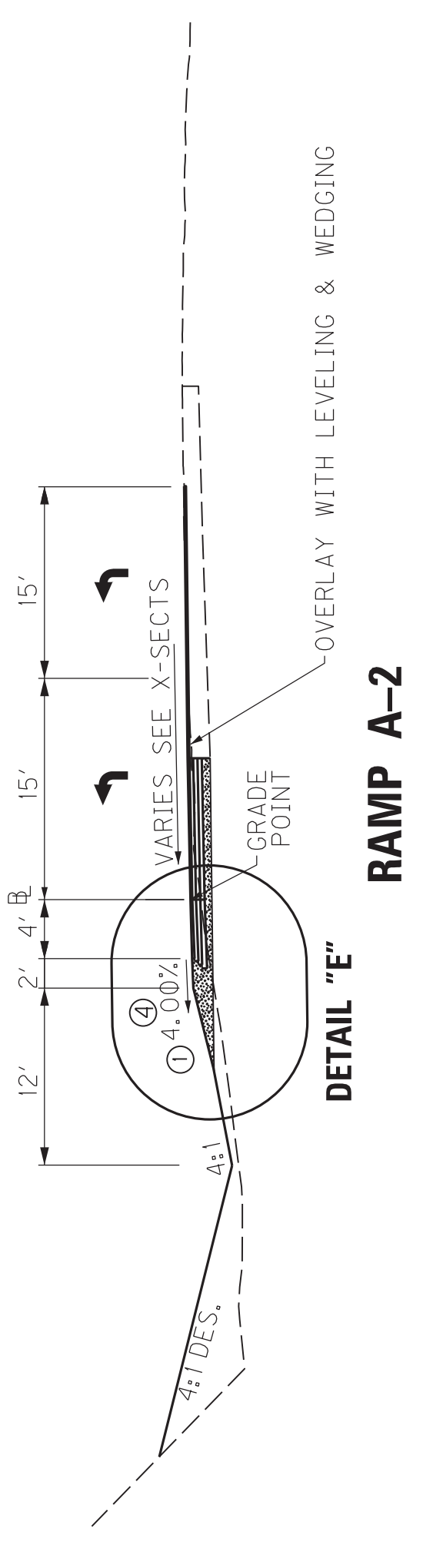
**RAMP A-1**  
**STA. 110 + 17.02 TO STA. 111 + 11.00**



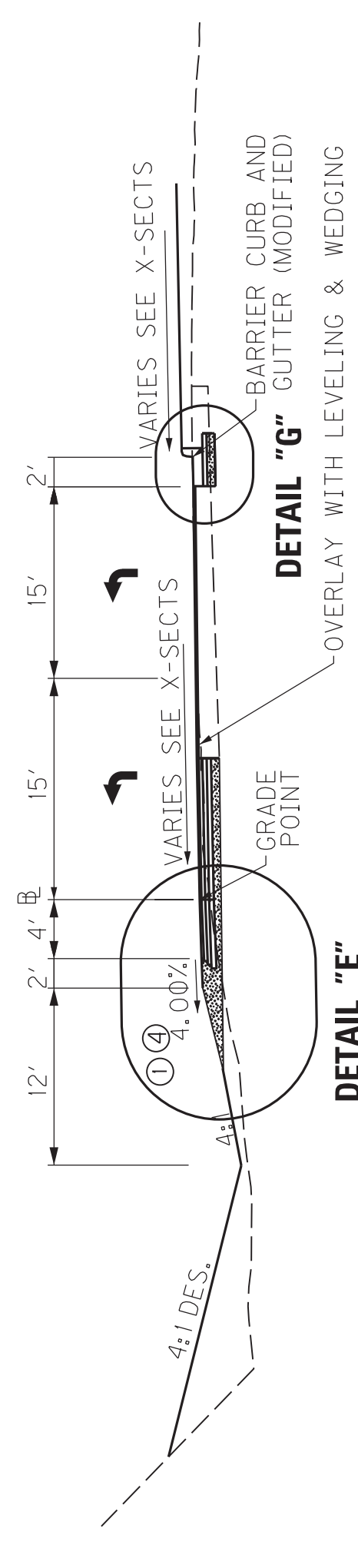
**RAMP A-1**  
**STA. 111 + 11.00 TO STA. 112 + 40.71**



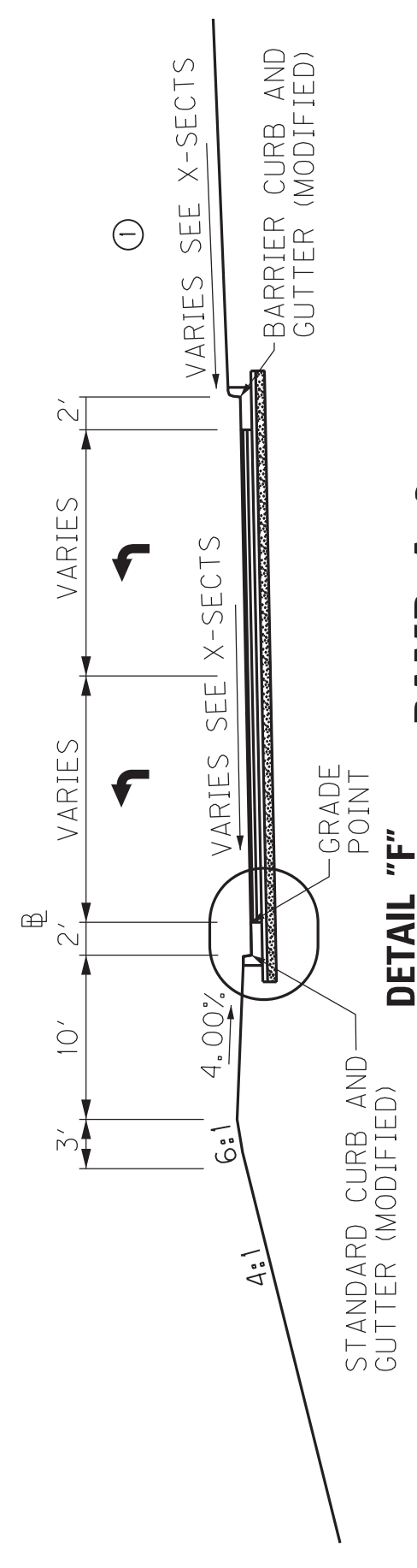
**RAMP A-1**  
**STA. 111 + 80.00 TO STA. 112 + 40.71**



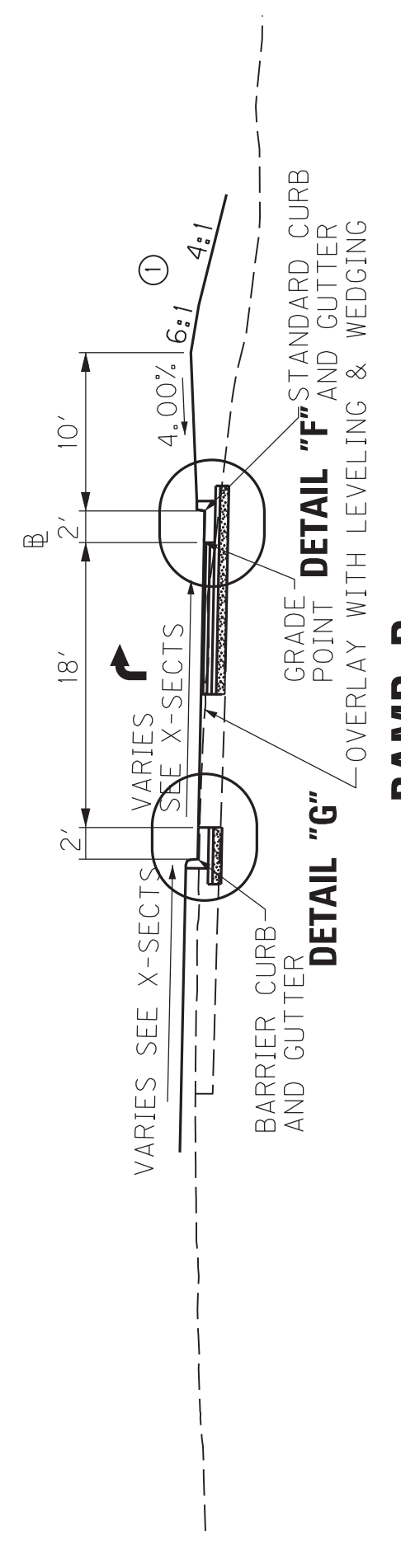
**RAMP A-2**  
**STA. 209 + 67.04 TO STA. 210 + 05.03**



**RAMP A-2**  
**STA. 210 + 05.03 TO STA. 210 + 15.60**



**RAMP A-2**  
**STA. 210 + 15.60 TO STA. 211 + 60.61**



**RAMP B**  
**STA. 20 + 00.00 TO STA. 21 + 40.00**

**NEW CONSTRUCTION  
GRADE, DRAJG & SURFACING  
- USING -**

**RAMP PAVEMENT:**

- 10.00" CRUSHED STONE BASE
- 4.00" CL3 ASPH. BASE 1,000 PG64-22
- 3.75" CL3 ASPH. BASE 1,000 PG64-22
- 3.25" CL3 ASPH. BASE 1,000 PG64-22
- 0.50" ASPHALT SCRATCH COURSE PG64-22

**1.50" SURFACE** - 1.50" CL3 ASPH. SURF. 0.50B PG76-22

**SHOULDERS:**  
VARIES: STANDARD CURB AND GUTTER (MODIFIED)

**OR**

**1.50" SURFACE** - 1.50" CL3 ASPH. SURF. 0.50B PG76-22

**21.50" BASE**

- 10.00" CRUSHED STONE BASE
- 4.00" CL3 ASPH. BASE 1,000 PG64-22
- 3.75" CL3 ASPH. BASE 1,000 PG64-22
- 3.25" CL3 ASPH. BASE 1,000 PG64-22
- 0.50" ASPHALT SCRATCH COURSE PG64-22

**PAVEMENT OVERLAY  
- USING -**

**RAMP PAVEMENT:**

**LEVEL & WEDGE PG64-22**

1.50" CL3 ASPH. SURF. 0.50B PG76-22

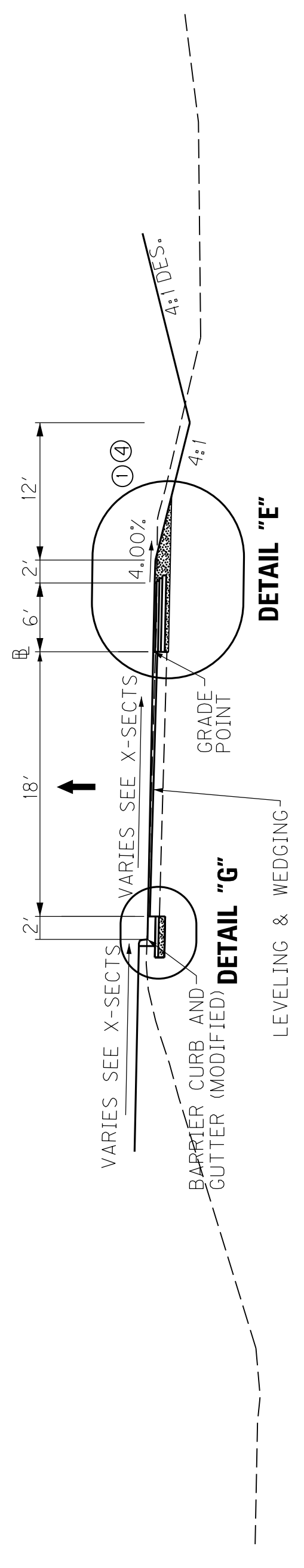
- ① SEE CROSS SECTIONS FOR CONSTRUCTION OUTSIDE THE SHARED USE PATH OR BERM.
- ② SHOULDER SHALL BE WIDENED 1' WHERE GUARDRAIL IS REQUIRED
- ③ SUPERELEVATED SHOULDERS SHALL BE CONSTRUCTED TO STANDARD SUPERELEVATION, EXCEPT NOT FLATTER THAN THE SLOPES INDICATED FOR NORMAL SHOULDERS.
- ④ ASPHALT SEAL REQUIRED FROM OUTSIDE EDGE OF PAVED SHOULDER TO A POINT 2' DOWN THE DITCH OR FILL SLOPE. TWO APPLICATIONS OF THE FOLLOWING:  
ASPHALT SEAL COAT - 2.4 LB/SY  
ASPHALT SEAL AGGREGATE - 20 LB/SY (SIZE NO. 8 OR 9M)

SCALE: NTS

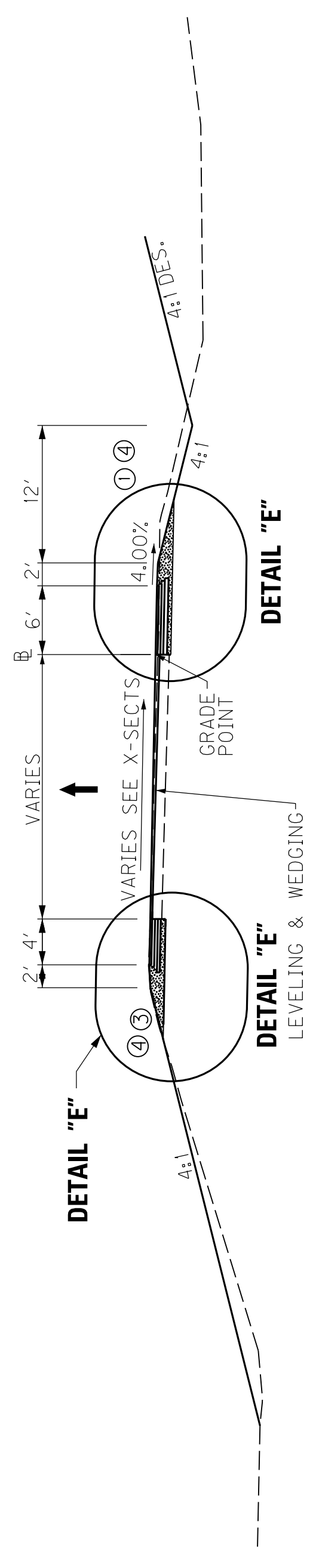
TYPICAL SECTIONS

# TYPICAL SECTIONS RAMPS

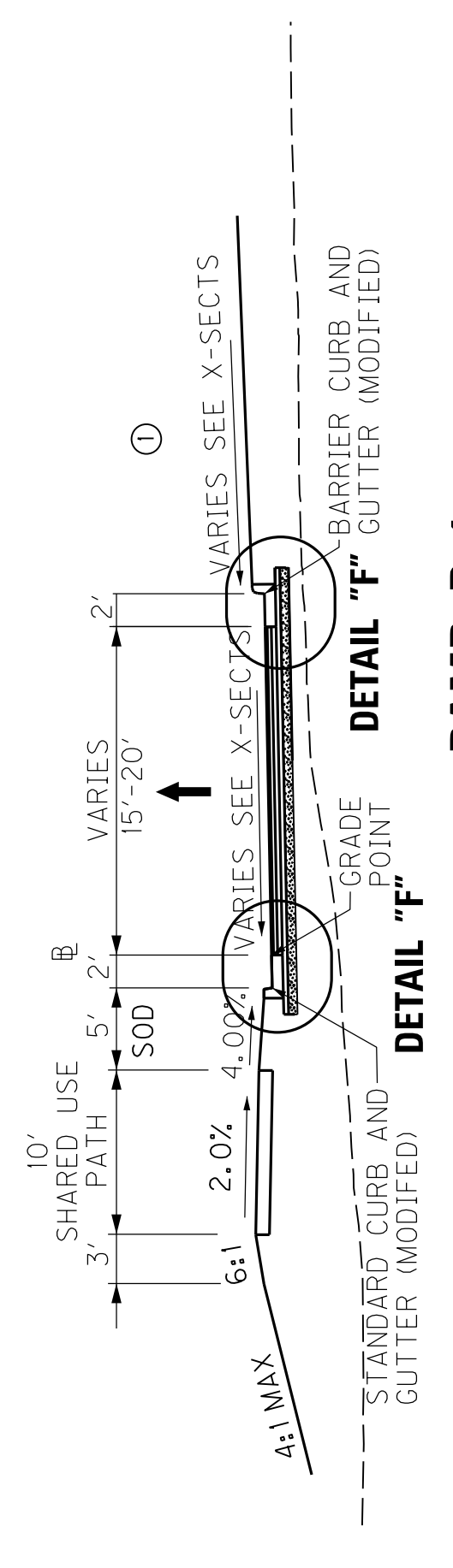
COUNTY OF	ITEM NO.	SHEET NO.
McCRACKEN	1-154.00	R2G



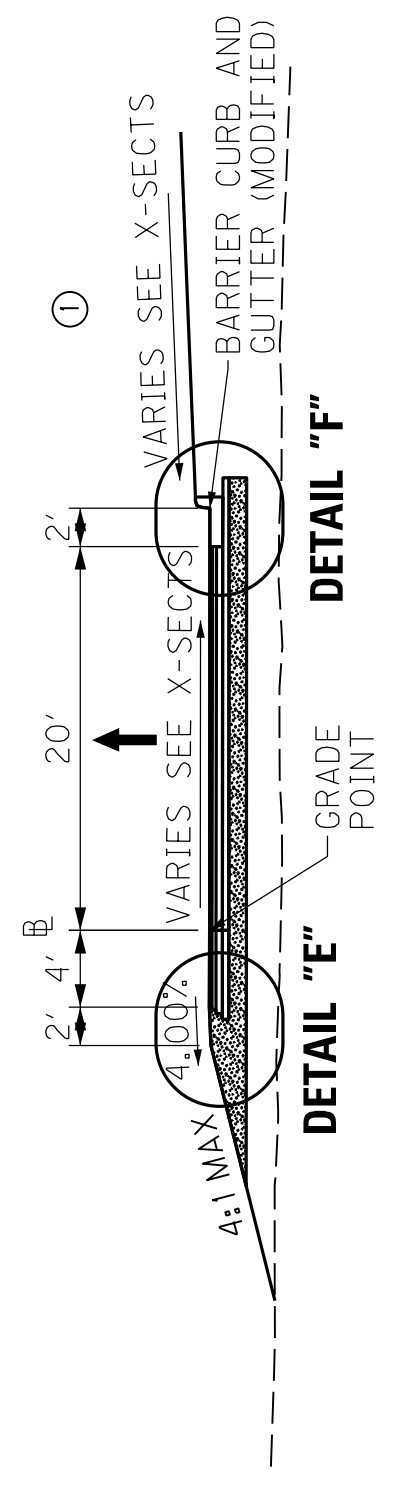
**RAMP B**  
**STA. 21 + 40.00 TO STA. 22 + 85.90**



**RAMP B**  
**STA. 22 + 85.90 TO STA. 23 + 58.82**



**RAMP B-1**  
**STA. 119 + 96.74 TO STA. 122 + 15.00**



**RAMP B-1**  
**STA. 122 + 15.00 TO STA. 122 + 96.92**

**NEW CONSTRUCTION  
GRADE, DRAIN & SURFACING**  
- USING -

**RAMP PAVEMENT**

- 21.50' BASE
  - 10.00" CRUSHED STONE BASE
  - 4.00" CL3 ASPH. BASE 1.00D PG64-22
  - 3.75" CL3 ASPH. BASE 1.00D PG64-22
  - 3.25" CL3 ASPH. BASE 1.00D PG64-22
  - 0.50" ASPHALT SCRATCH COURSE PG64-22

- 1.50' SURFACE
  - 1.50" CL3 ASPH. SURF. 0.50B PG76-22

**SHOULDERS**  
VARIABLES: STANDARD CURB AND GUTTER (MODIFIED)

OR

- 1.50' SURFACE
  - 1.50" CL3 ASPH. SURF. 0.50B PG76-22

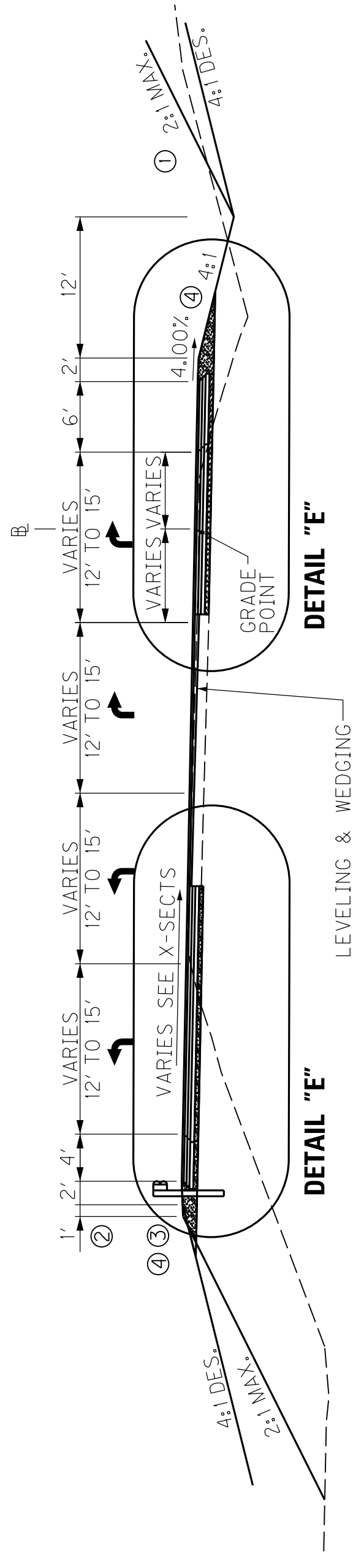
- 21.50' BASE
  - 10.00" CRUSHED STONE BASE
  - 4.00" CL3 ASPH. BASE 1.00D PG64-22
  - 3.75" CL3 ASPH. BASE 1.00D PG64-22
  - 3.25" CL3 ASPH. BASE 1.00D PG64-22
  - 0.50" ASPHALT SCRATCH COURSE PG64-22

**PAVEMENT OVERLAY**  
- USING -

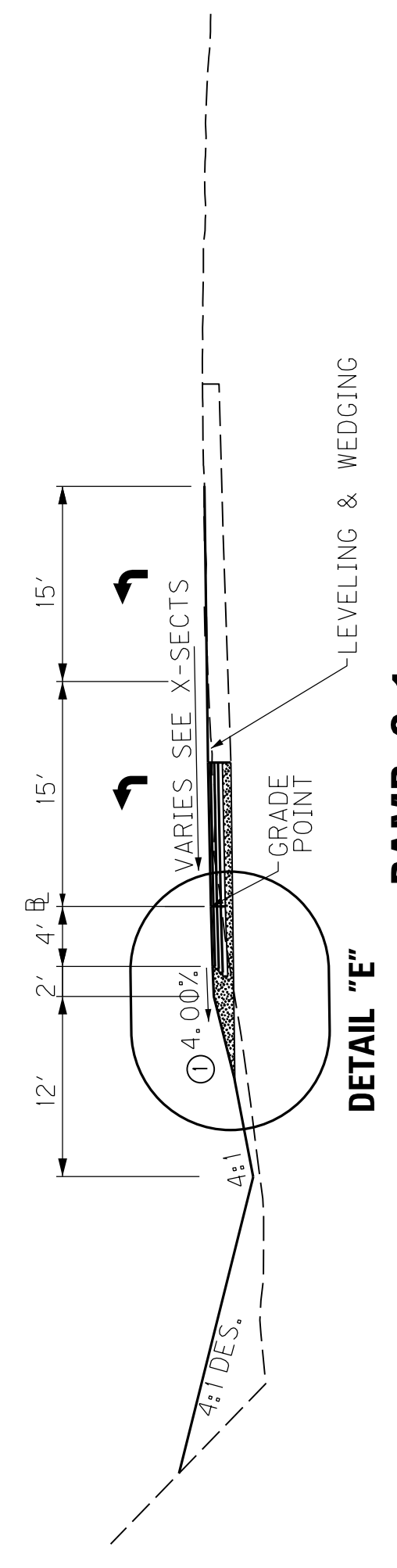
**RAMP PAVEMENT**

- LEVEL & WEDGE PG64-22
- 1.50" CL3 ASPH. SURF. 0.50B PG76-22

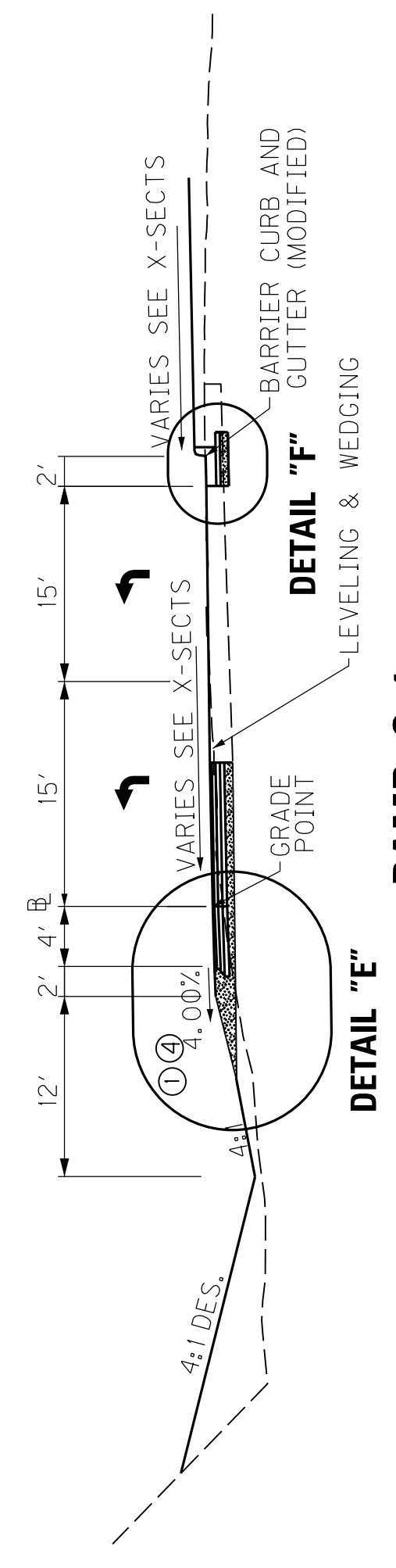
- ① SEE CROSS SECTIONS FOR CONSTRUCTION OUTSIDE THE SHARED USE PATH OR BERM.
- ② SHOULDER SHALL BE WIDENED 1' WHERE GUARDRAIL IS REQUIRED
- ③ SUPERELEVATED SHOULDERS SHALL BE CONSTRUCTED TO STANDARD SUPERELEVATION EXCEPT NOT FLATTER THAN THE SLOPES INDICATED FOR NORMAL SHOULDERS.
- ④ ASPHALT SEAL REQUIRED FROM OUTSIDE EDGE OF PAVED SHOULDER TO A POINT 2' DOWN THE DITCH OR FILL SLOPE. TWO APPLICATIONS OF THE FOLLOWING:  
 ASPHALT SEAL COAT - 2.4 LB/SY  
 ASPHALT SEAL AGGREGATE - 20 LB/SY  
 (SIZE NO. 8 OR 9M)



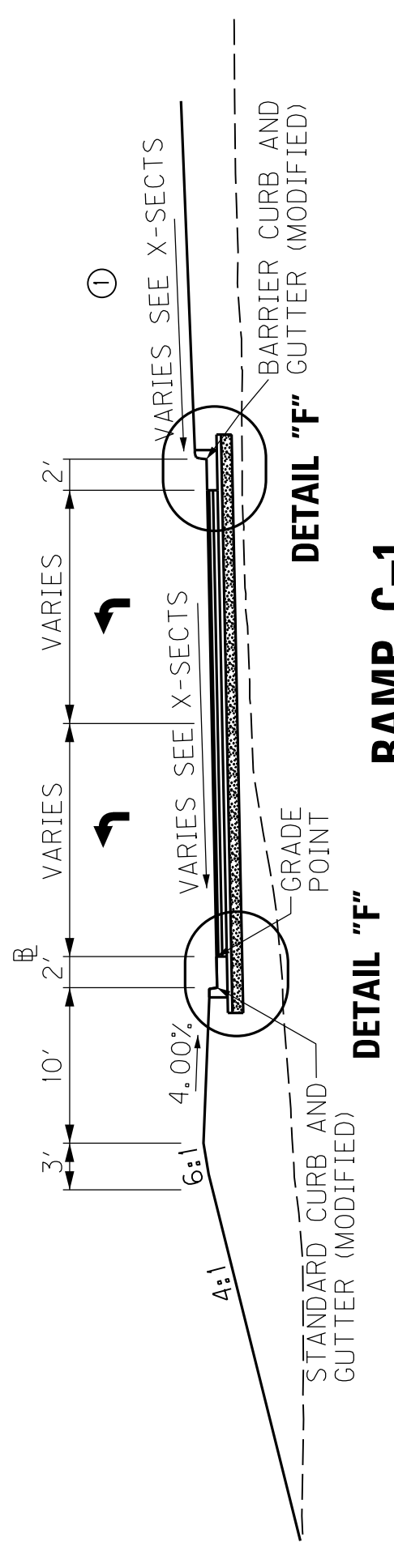
**RAMP C**  
**STA. 33 + 84.57 TO STA. 36 + 88.24**



**RAMP C-1**  
**STA. 136 + 88.24 TO STA. 137 + 26.23**



**RAMP C-1**  
**STA. 137 + 26.23 TO STA. 137 + 88.00**



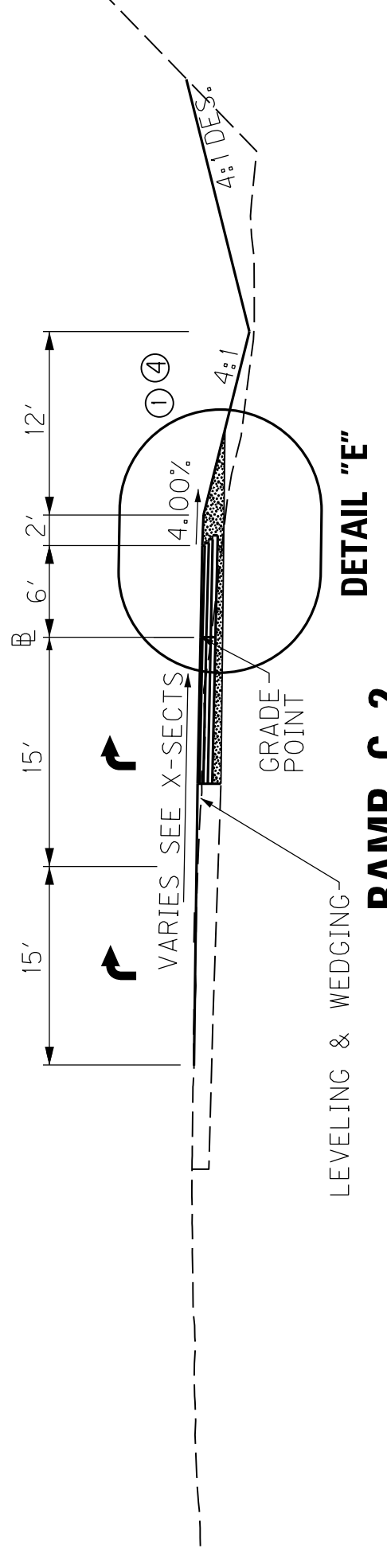
**RAMP C-1**  
**STA. 137 + 88.00 TO STA. 138 + 67.66**



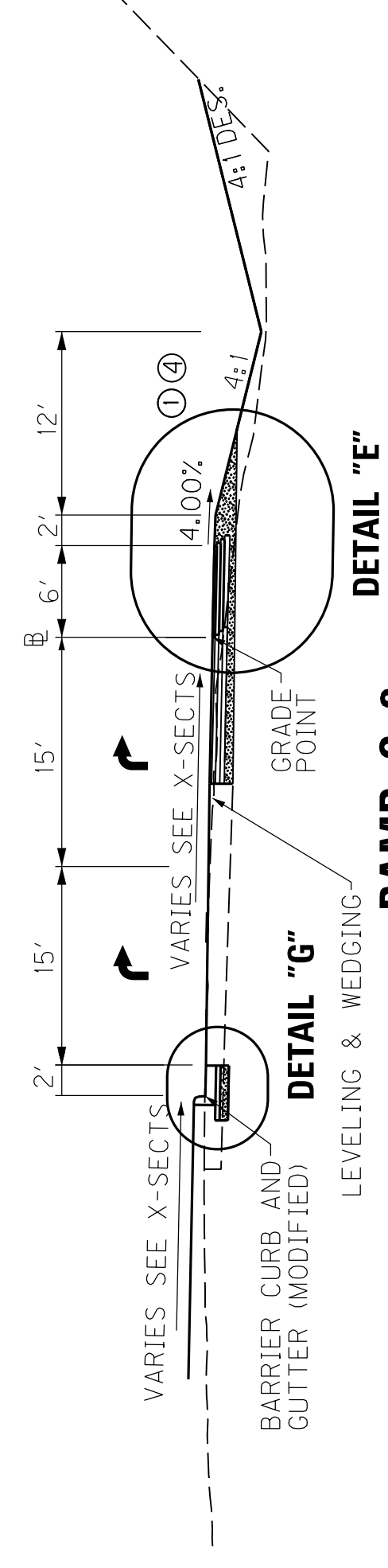


# TYPICAL SECTIONS RAMPS

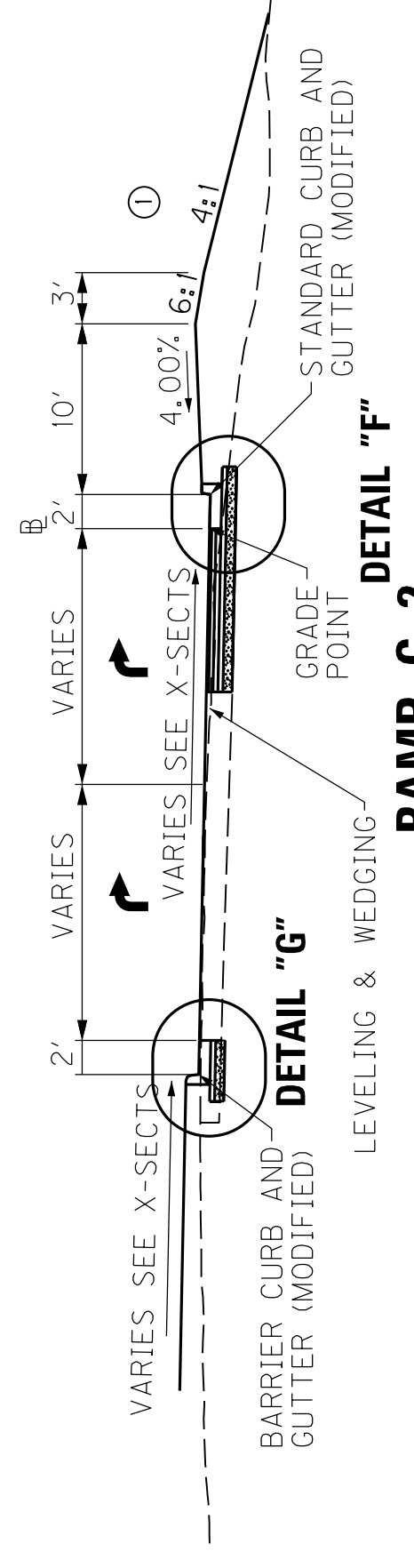
COUNTY OF	ITEM NO.	SHEET NO.
McCRACKEN	I-154-00	R2H



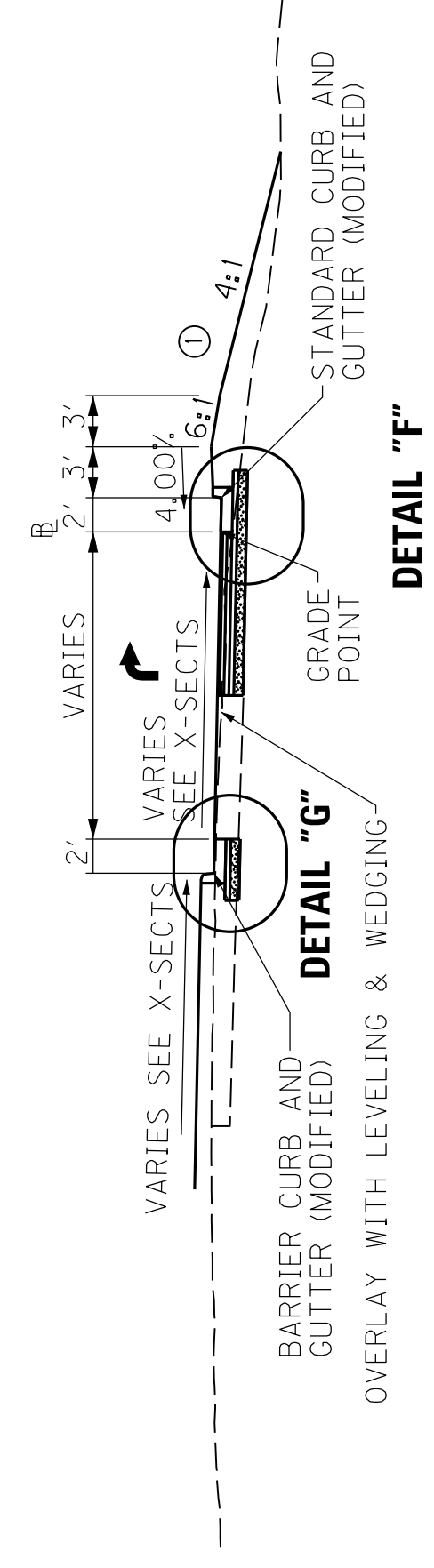
**RAMP C-2**  
**STA. 236 + 88.24 TO STA. 237 + 38.22**



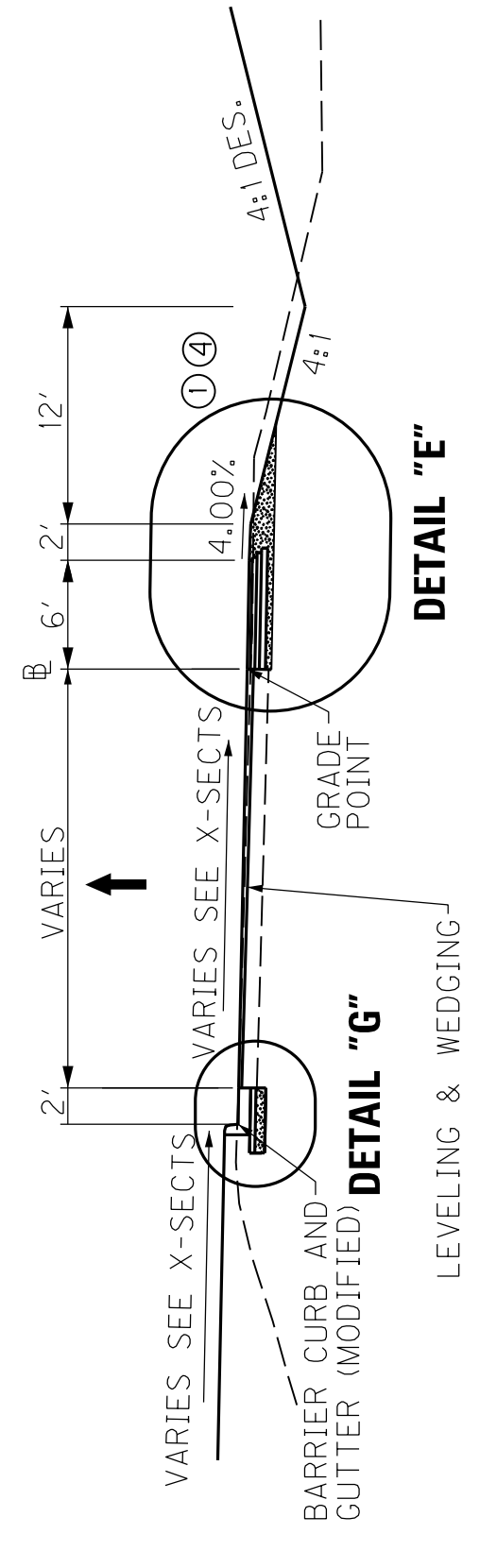
**RAMP C-2**  
**STA. 237 + 38.22 TO STA. 238 + 00.00**



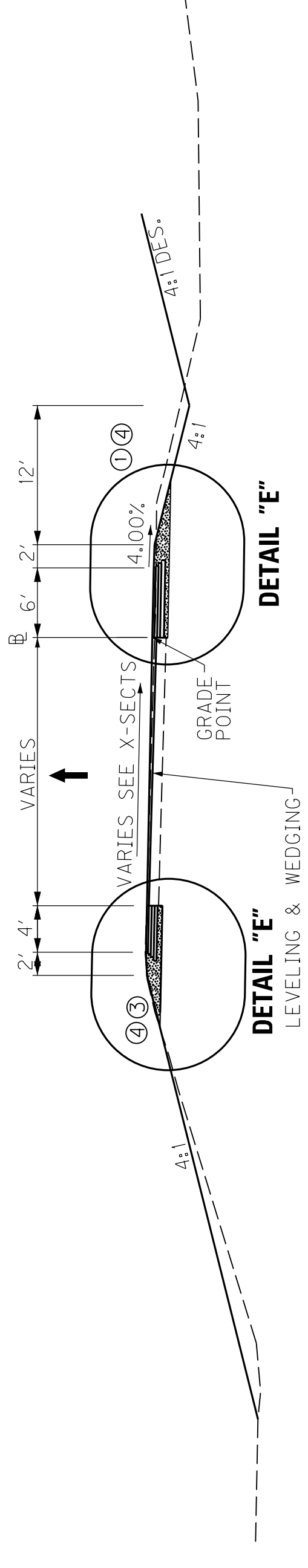
**RAMP C-2**  
**STA. 238 + 00.00 TO STA. 239 + 23.40**



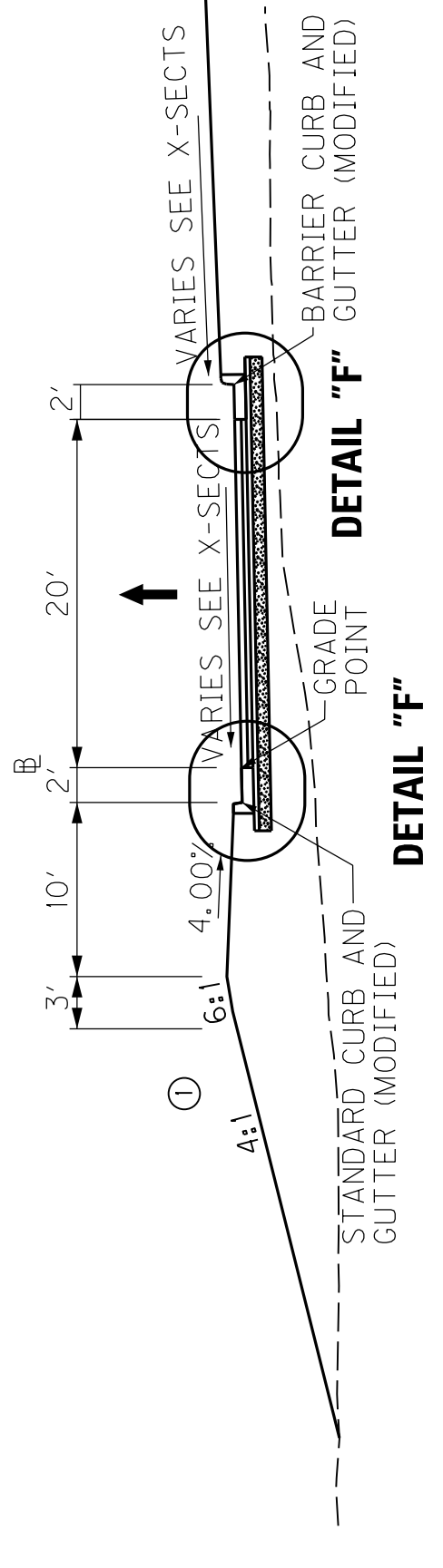
**RAMP D**  
**STA. 40 + 00.00 TO STA. 41 + 06.00**



**RAMP D**  
**STA. 41 + 06.00 TO STA. 42 + 86.44**



**RAMP D**  
**STA. 42 + 86.44 TO STA. 44 + 30.62**



**RAMP D-1**  
**STA. 140 + 28.78 TO STA. 143 + 00.00**

- NEW CONSTRUCTION GRADE, DRAIN & SURFACING - USING -**
- RAMP PAVEMENT**
- 21.50" BASE - [10.00" CRUSHED STONE BASE  
4.00" CL3 ASPH. BASE 1.000 PG64-22  
3.75" CL3 ASPH. BASE 1.000 PG64-22  
3.25" CL3 ASPH. BASE 1.000 PG64-22  
0.50" ASPHALT SCRATCH COURSE PG64-22
  - 1.50" SURFACE - [1.50" CL3 ASPH. SURF. 0.50B PG76-22
- SHOULDERS:  
VARIES: STANDARD CURB AND GUTTER (MODIFIED)
- OR
- 1.50" SURFACE - [1.50" CL3 ASPH. SURF. 0.50B PG76-22
  - 21.50" BASE - [10.00" CRUSHED STONE BASE  
4.00" CL3 ASPH. BASE 1.000 PG64-22  
3.75" CL3 ASPH. BASE 1.000 PG64-22  
3.25" CL3 ASPH. BASE 1.000 PG64-22  
0.50" ASPHALT SCRATCH COURSE PG64-22
- PAVEMENT OVERLAY - USING -**
- RAMP PAVEMENT**
- LEVEL & WEDGE PG64-22  
1.50" CL3 ASPH. SURF. 0.50B PG76-22

- ① SEE CROSS SECTIONS FOR CONSTRUCTION OUTSIDE THE SHARED USE PATH OR BERM.
- ② SHOULDER SHALL BE WIDENED 1' WHERE GUARDRAIL IS REQUIRED
- ③ SUPERELEVATED SHOULDERS SHALL BE CONSTRUCTED TO STANDARD SUPERELEVATION, EXCEPT NOT FLATTER THAN THE SLOPES INDICATED FOR NORMAL SHOULDERS.
- ④ ASPHALT SEAL REQUIRED FROM OUTSIDE EDGE OF PAVED SHOULDER TO A POINT 2' DOWN THE DITCH OR FILL SLOPE. TWO APPLICATIONS OF THE FOLLOWING:  
ASPHALT SEAL COAT - 2.4 LB/SY  
ASPHALT SEAL AGGREGATE - 20 LB/SY (SIZE NO. 8 OR 9M)

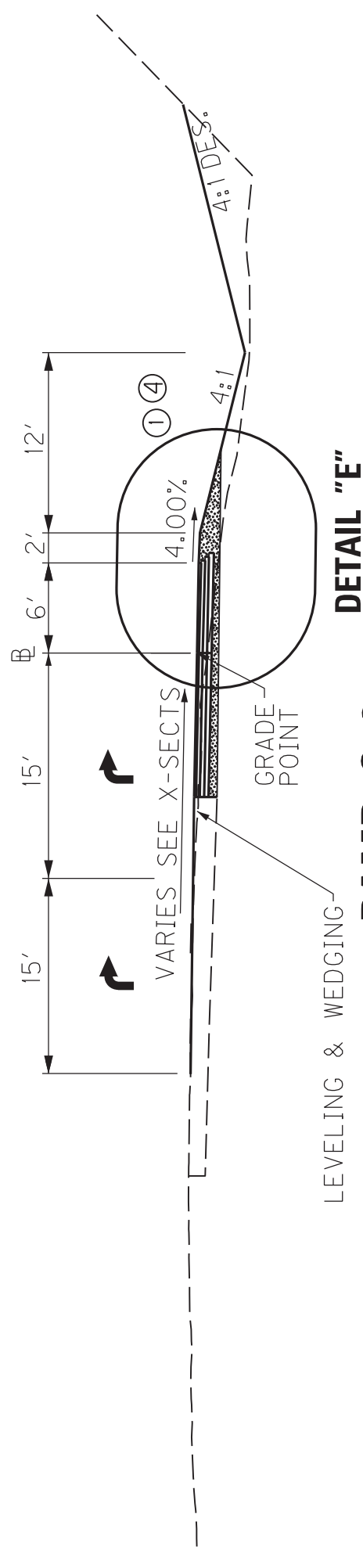
SCALE: NTS

TYPICAL SECTIONS

# TYPICAL SECTIONS RAMPS

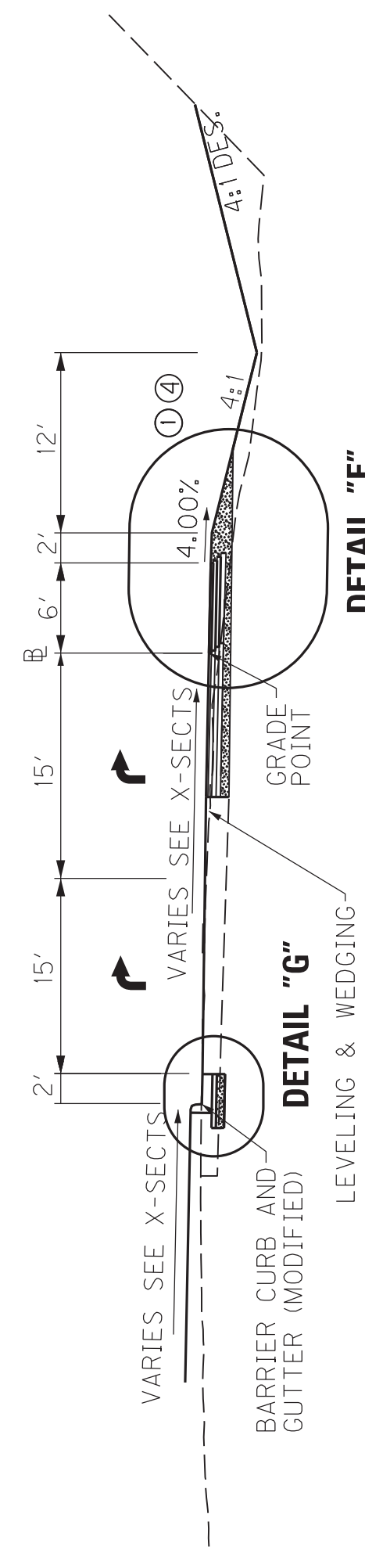
COUNTY OF	ITEM NO.	SHEET NO.
McCRACKEN	I-154-00	R2H

REVISED 11-30-18



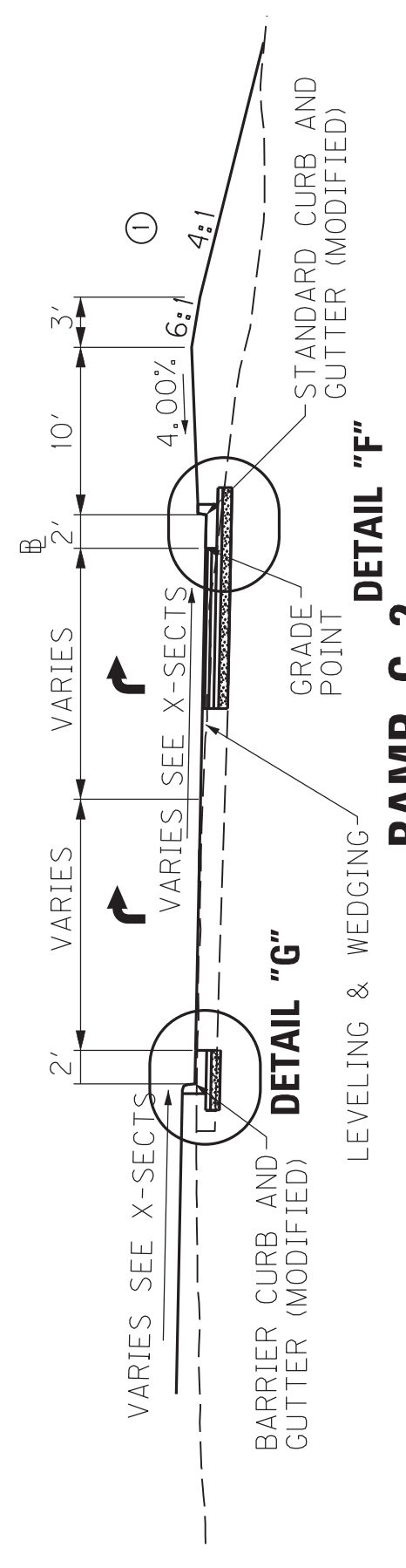
### RAMP C-2

STA. 236 + 88.24 TO STA. 237 + 38.22



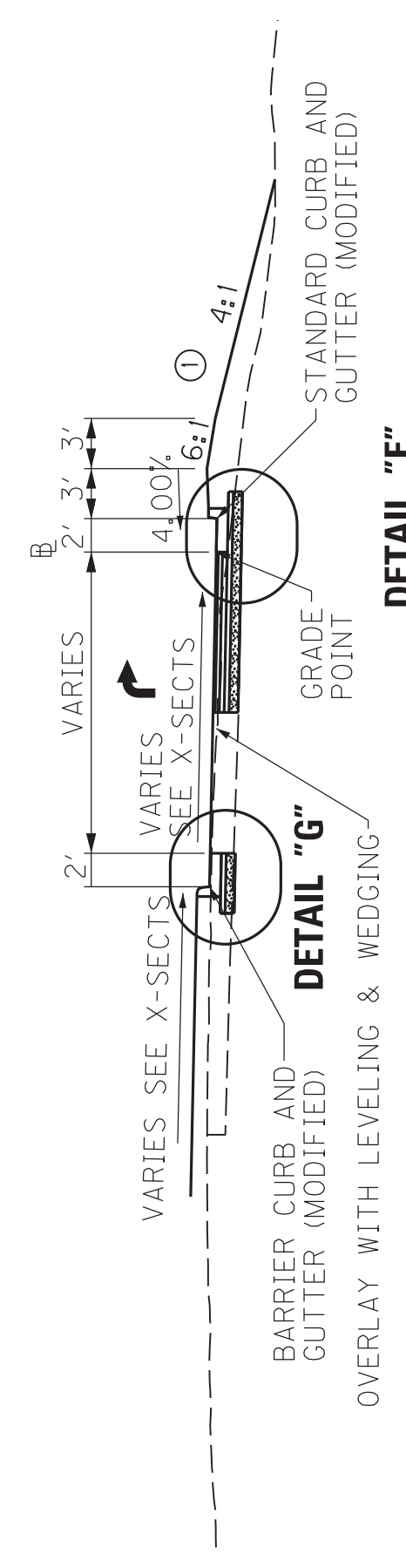
### RAMP C-2

STA. 237 + 38.22 TO STA. 238 + 00.00



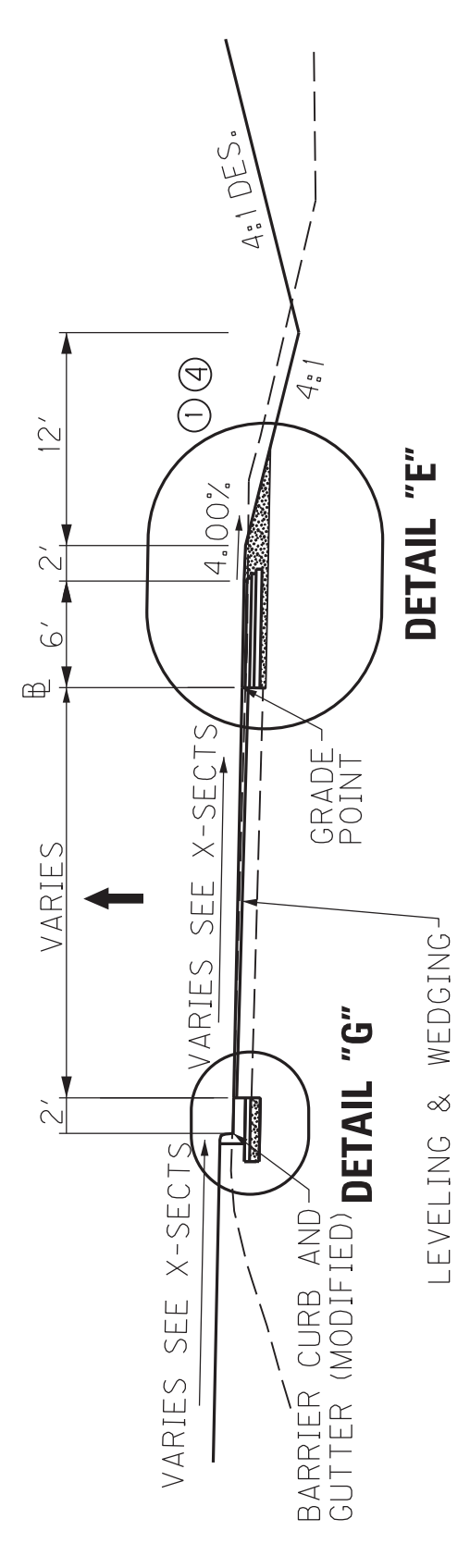
### RAMP C-2

STA. 238 + 00.00 TO STA. 239 + 23.40



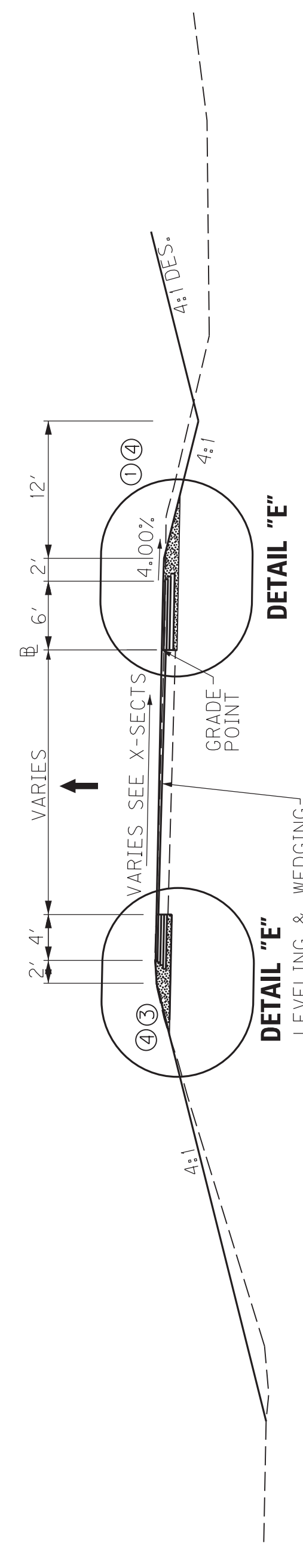
### RAMP D

STA. 40 + 00.00 TO STA. 41 + 06.00



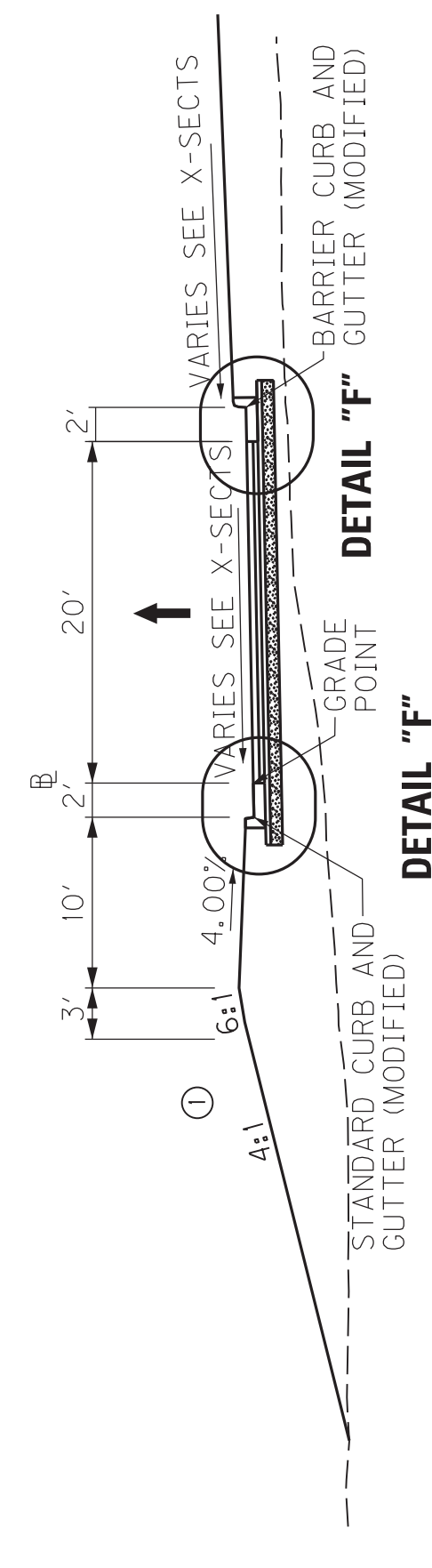
### RAMP D

STA. 41 + 06.00 TO STA. 42 + 86.44



### RAMP D

STA. 42 + 86.44 TO STA. 44 + 30.62



### RAMP D-1

STA. 140 + 28.78 TO STA. 143 + 00.00

NEW CONSTRUCTION  
GRADE, DRAIN & SURFACING  
- USING -

#### RAMP PAVEMENT

- 21.50" BASE
  - 10.00" CRUSHED STONE BASE
  - 4.00" CL3 ASPH. BASE 1.00D PG64-22
  - 3.75" CL3 ASPH. BASE 1.00D PG64-22
  - 3.25" CL3 ASPH. BASE 1.00D PG64-22
  - 0.50" ASPHALT SCRATCH COURSE PG64-22

- 1.50" SURFACE
  - 1.50" CL3 ASPH. SURF. 0.50B PG76-22

SHOULDERS  
VARIES: STANDARD CURB AND GUTTER (MODIFIED)

OR

- 1.50" SURFACE
  - 1.50" CL3 ASPH. SURF. 0.50B PG76-22

- 21.50" BASE
  - 10.00" CRUSHED STONE BASE
  - 4.00" CL3 ASPH. BASE 1.00D PG64-22
  - 3.75" CL3 ASPH. BASE 1.00D PG64-22
  - 3.25" CL3 ASPH. BASE 1.00D PG64-22
  - 0.50" ASPHALT SCRATCH COURSE PG64-22

PAVEMENT OVERLAY  
- USING -

#### RAMP PAVEMENT

- LEVELING & WEDGING PG64-22
- 1.50" CL3 ASPH. SURF. 0.50B PG76-22

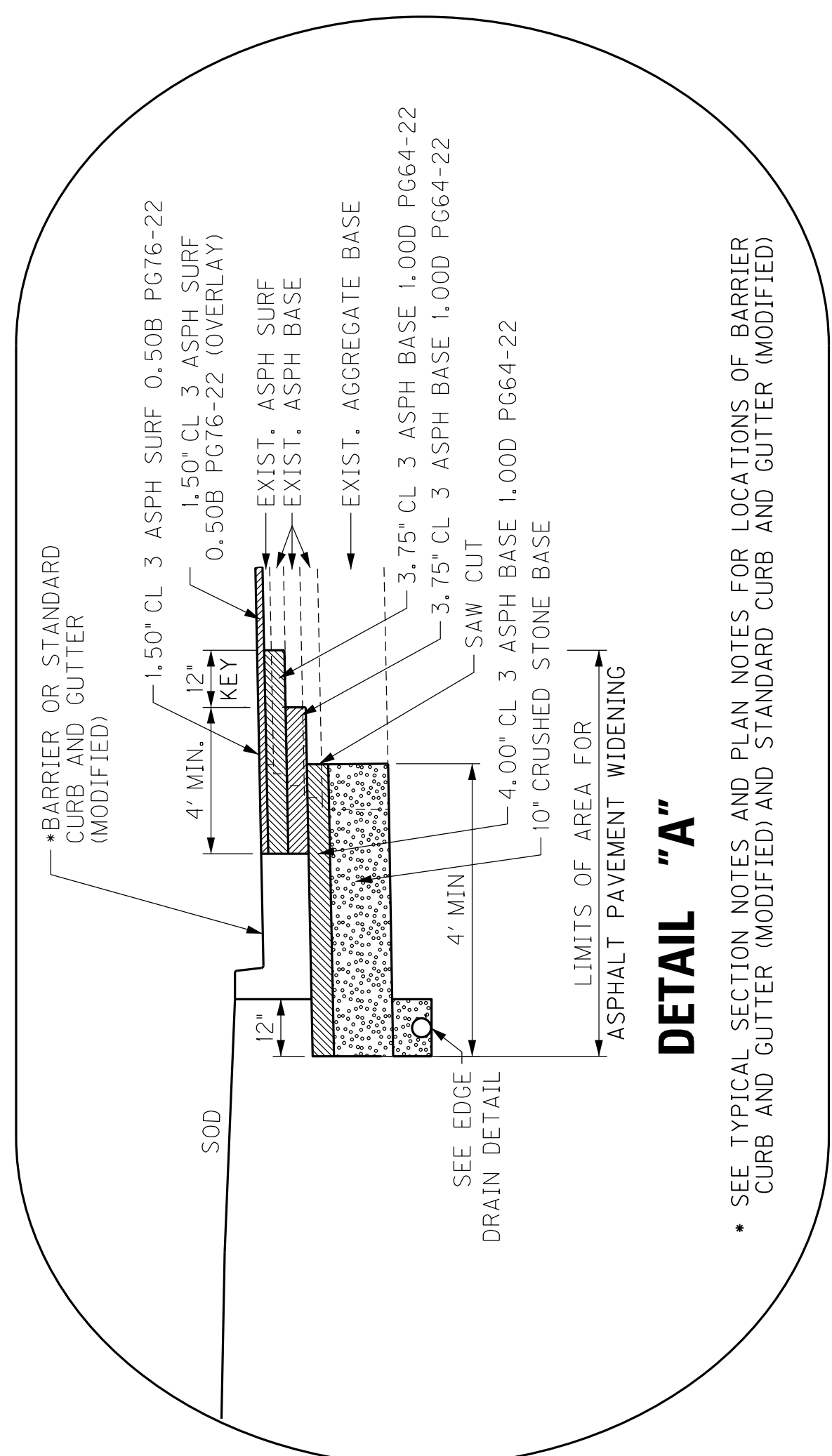
- ① SEE GROSS SECTIONS FOR CONSTRUCTION OUTSIDE THE SHARED USE PATH OR BERM.
- ② SHOULDER SHALL BE WIDENED 1' WHERE GUARDRAIL IS REQUIRED
- ③ SUPERELEVATED SHOULDERS SHALL BE CONSTRUCTED TO STANDARD SUPERELEVATION, EXCEPT NOT FLATTER THAN THE SLOPES INDICATED FOR NORMAL SHOULDERS.
- ④ ASPHALT SEAL REQUIRED FROM OUTSIDE EDGE OF PAVED SHOULDER TO A POINT 2' DOWN THE DITCH OR FILL SLOPE. TWO APPLICATIONS OF THE FOLLOWING:  
 ASPHALT SEAL COAT - 2.4 LB/SY  
 ASPHALT SEAL AGGREGATE - 20 LB/SY  
 (SIZE NO. 8 OR 9M)

SCALE: NTS

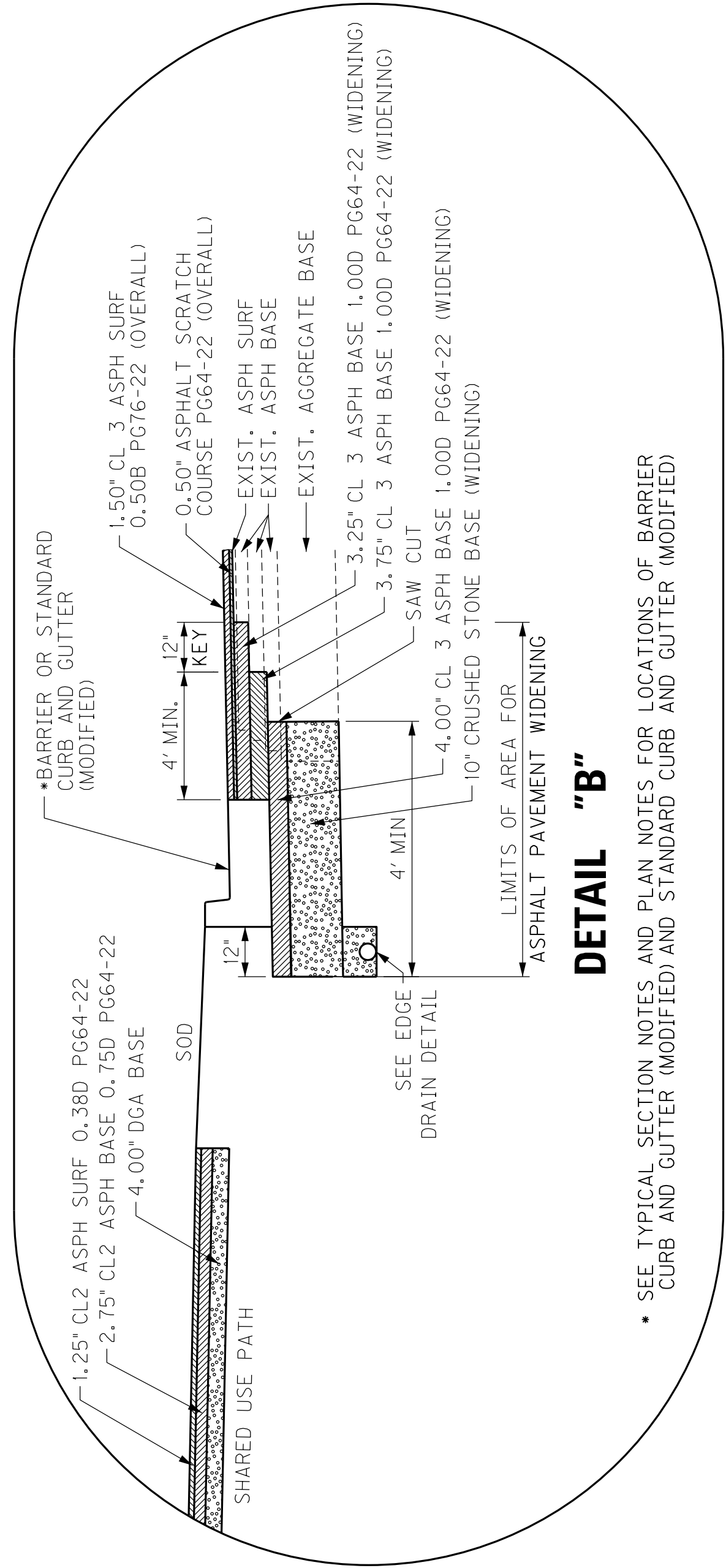
TYPICAL SECTIONS

# TYPICAL SECTIONS DETAILS

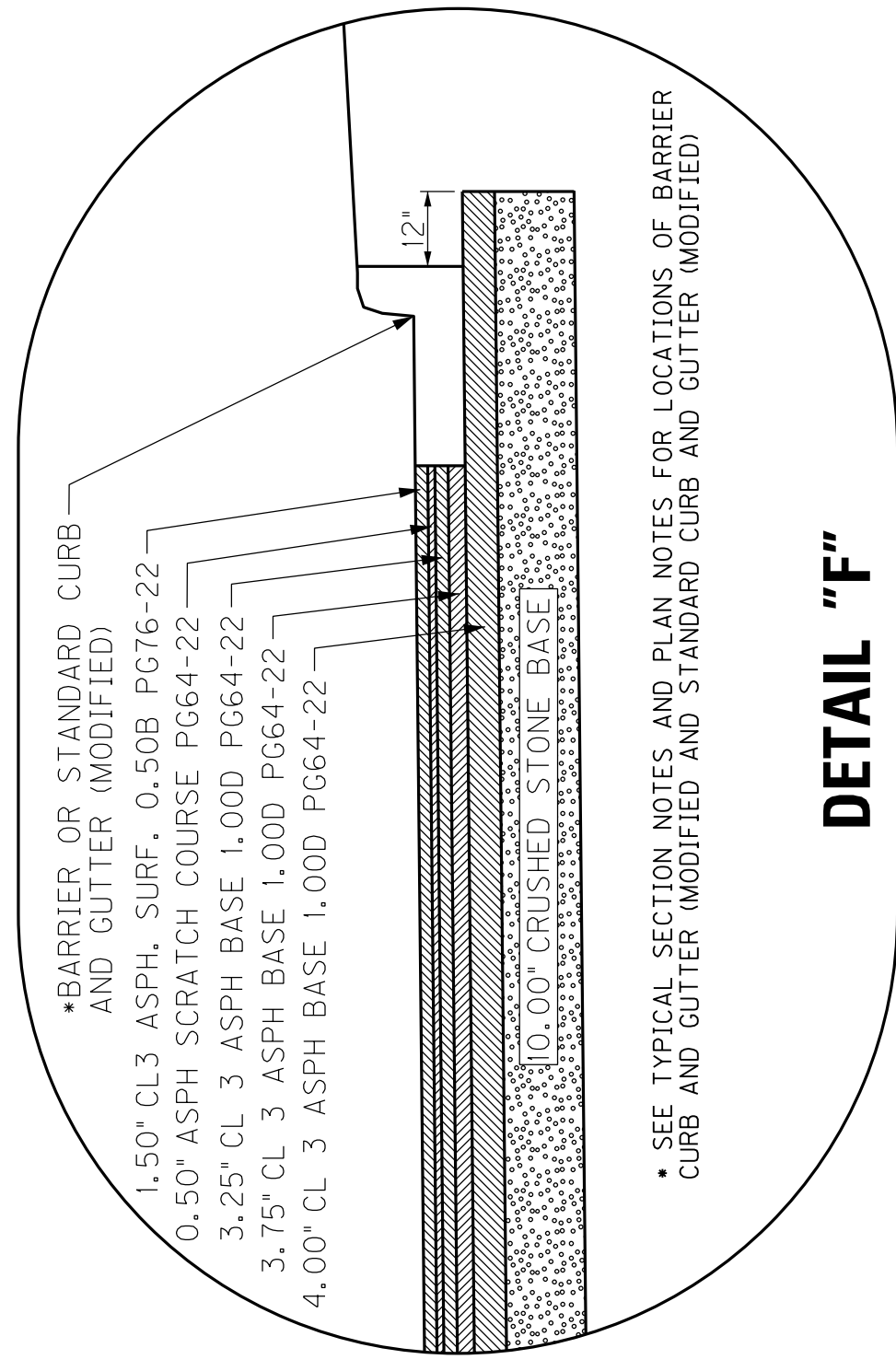
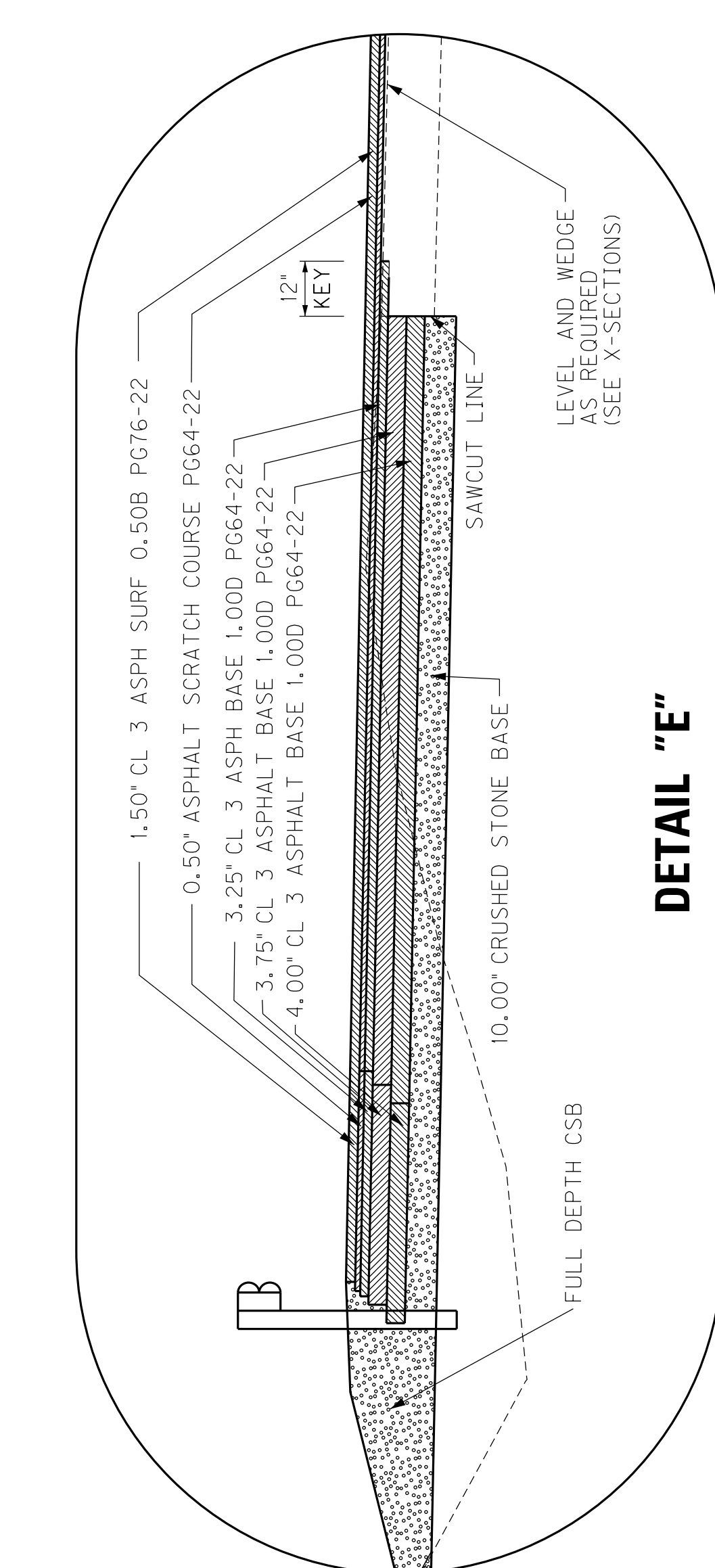
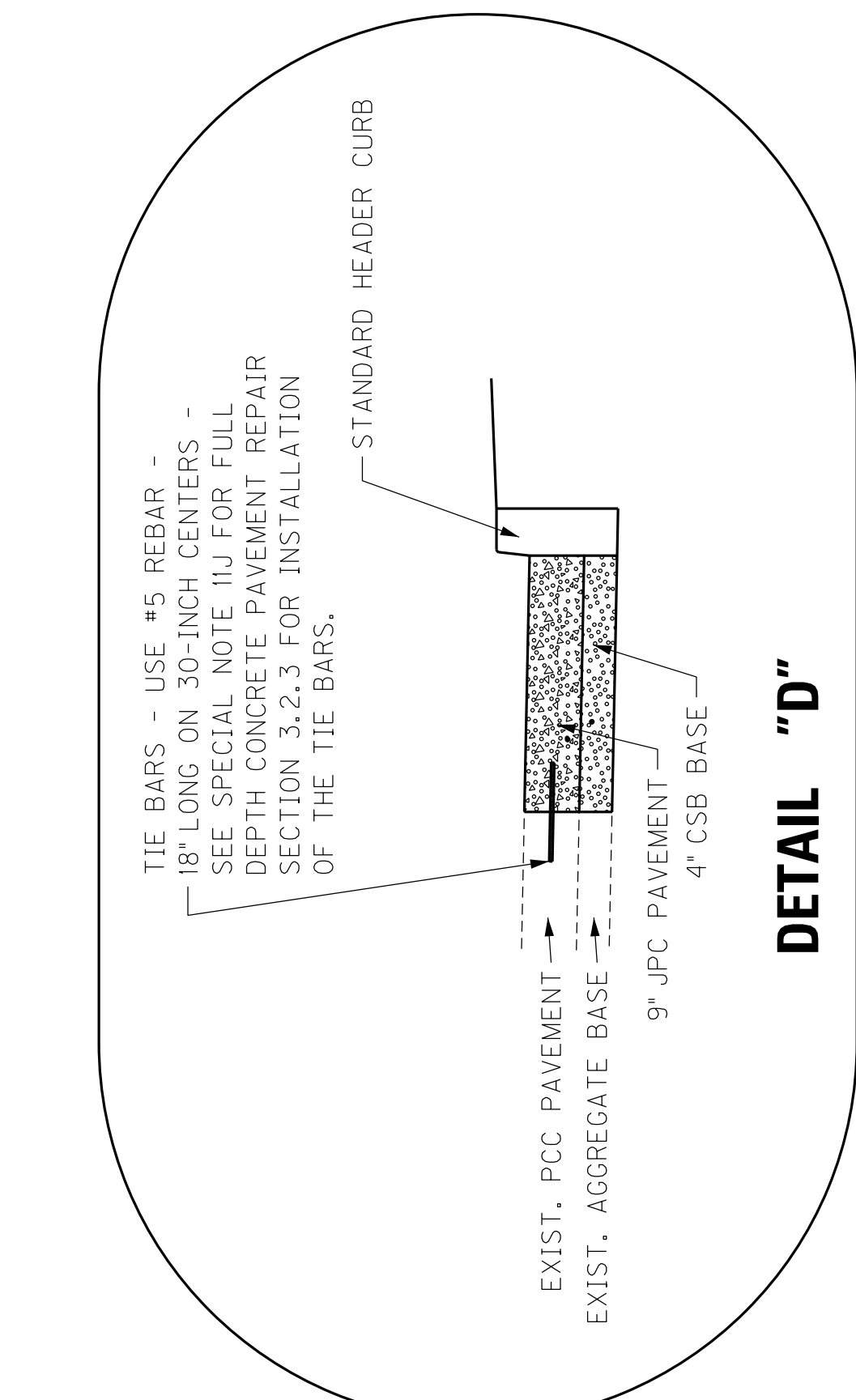
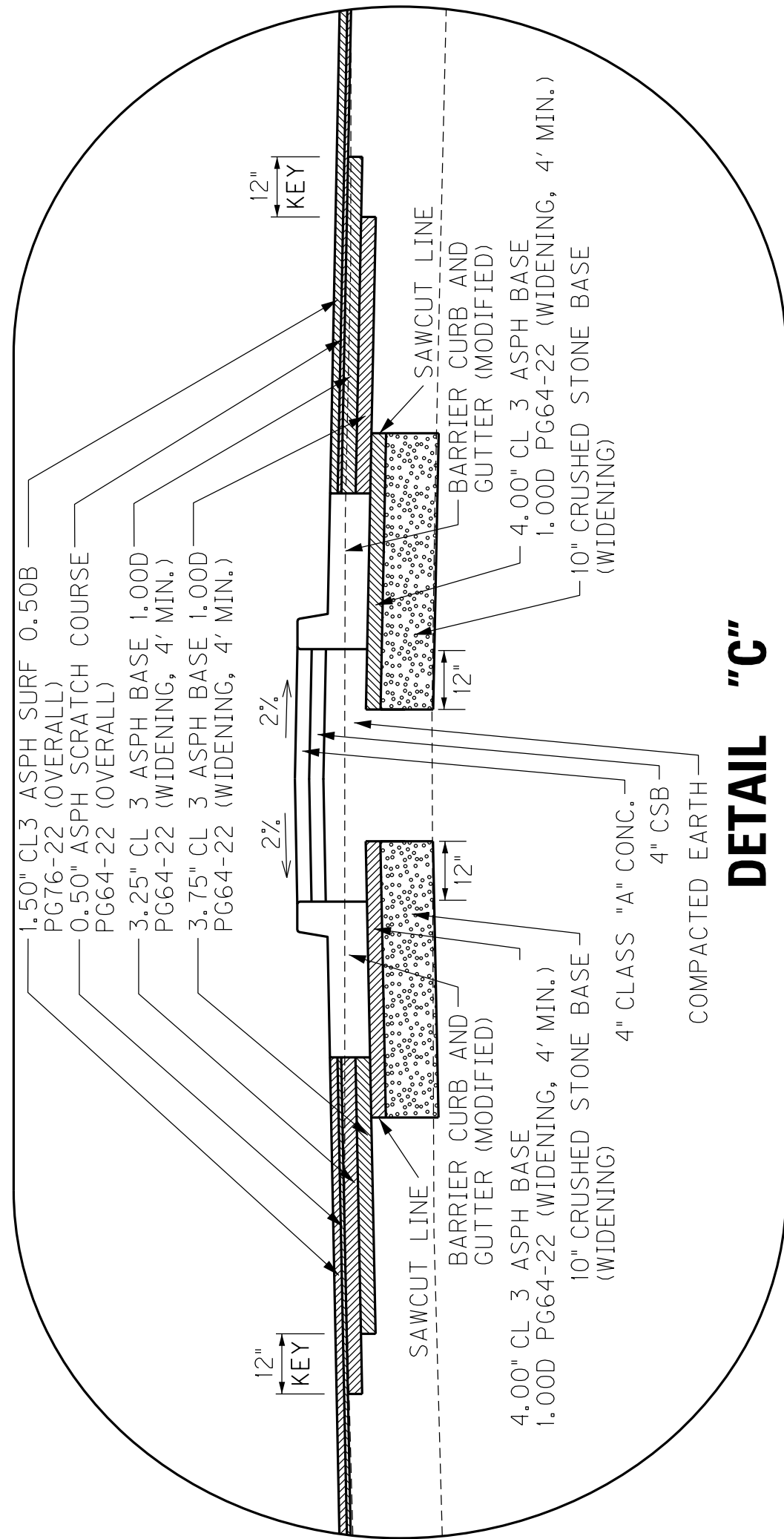
COUNTY OF	ITEM NO.	SHEET NO.
McCRACKEN	I-154.00	R21



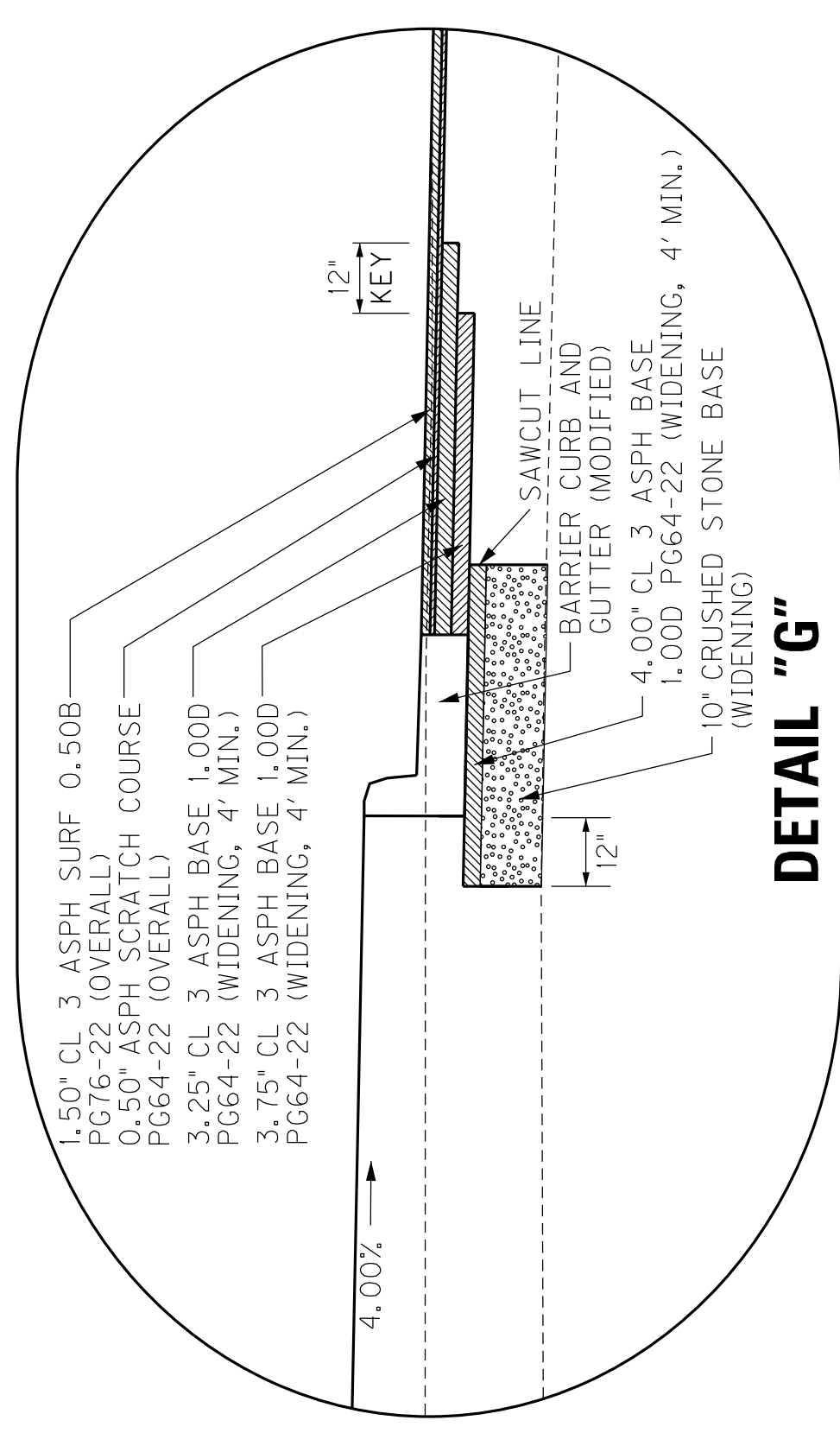
• SEE TYPICAL SECTION NOTES AND PLAN NOTES FOR LOCATIONS OF BARRIER CURB AND GUTTER (MODIFIED) AND STANDARD CURB AND GUTTER (MODIFIED)



• SEE TYPICAL SECTION NOTES AND PLAN NOTES FOR LOCATIONS OF BARRIER CURB AND GUTTER (MODIFIED) AND STANDARD CURB AND GUTTER (MODIFIED)



• SEE TYPICAL SECTION NOTES AND PLAN NOTES FOR LOCATIONS OF BARRIER CURB AND GUTTER (MODIFIED) AND STANDARD CURB AND GUTTER (MODIFIED)

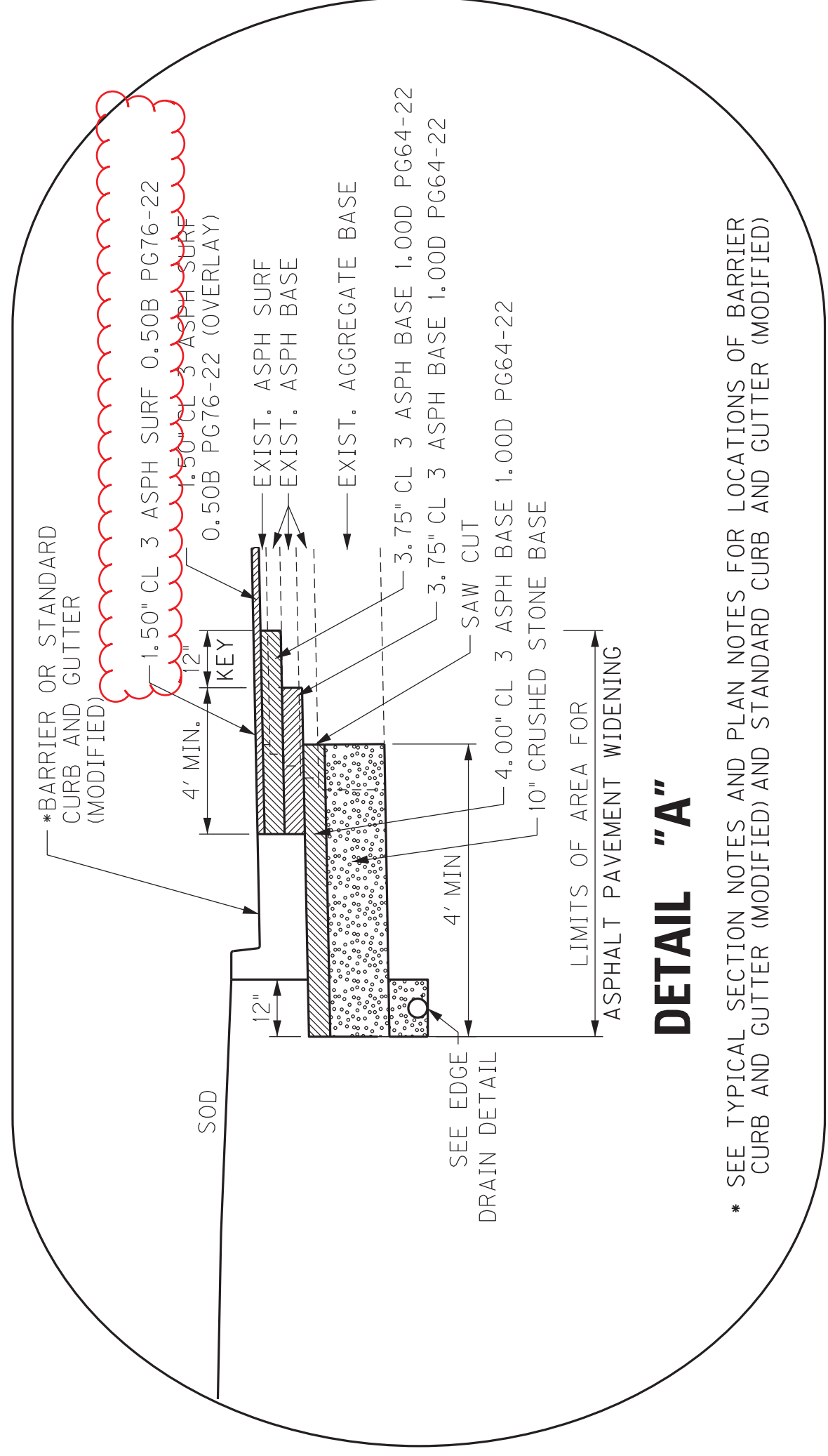


• SEE TYPICAL SECTION NOTES AND PLAN NOTES FOR LOCATIONS OF BARRIER CURB AND GUTTER (MODIFIED) AND STANDARD CURB AND GUTTER (MODIFIED)

# TYPICAL SECTIONS DETAILS

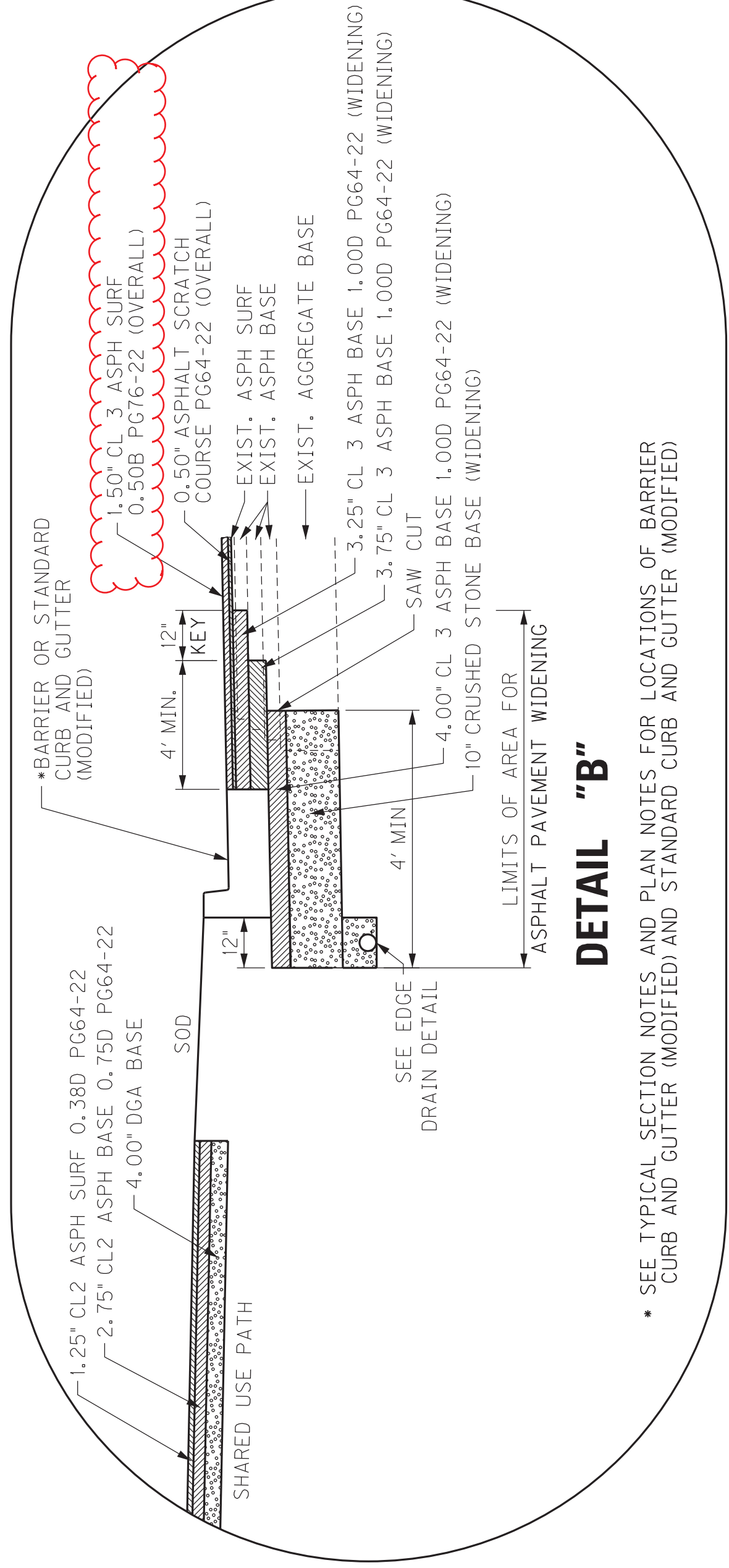
COUNTY OF	ITEM NO.	SHEET NO.
McCRACKEN	I-154.00	R21

REVISED 11-30-18



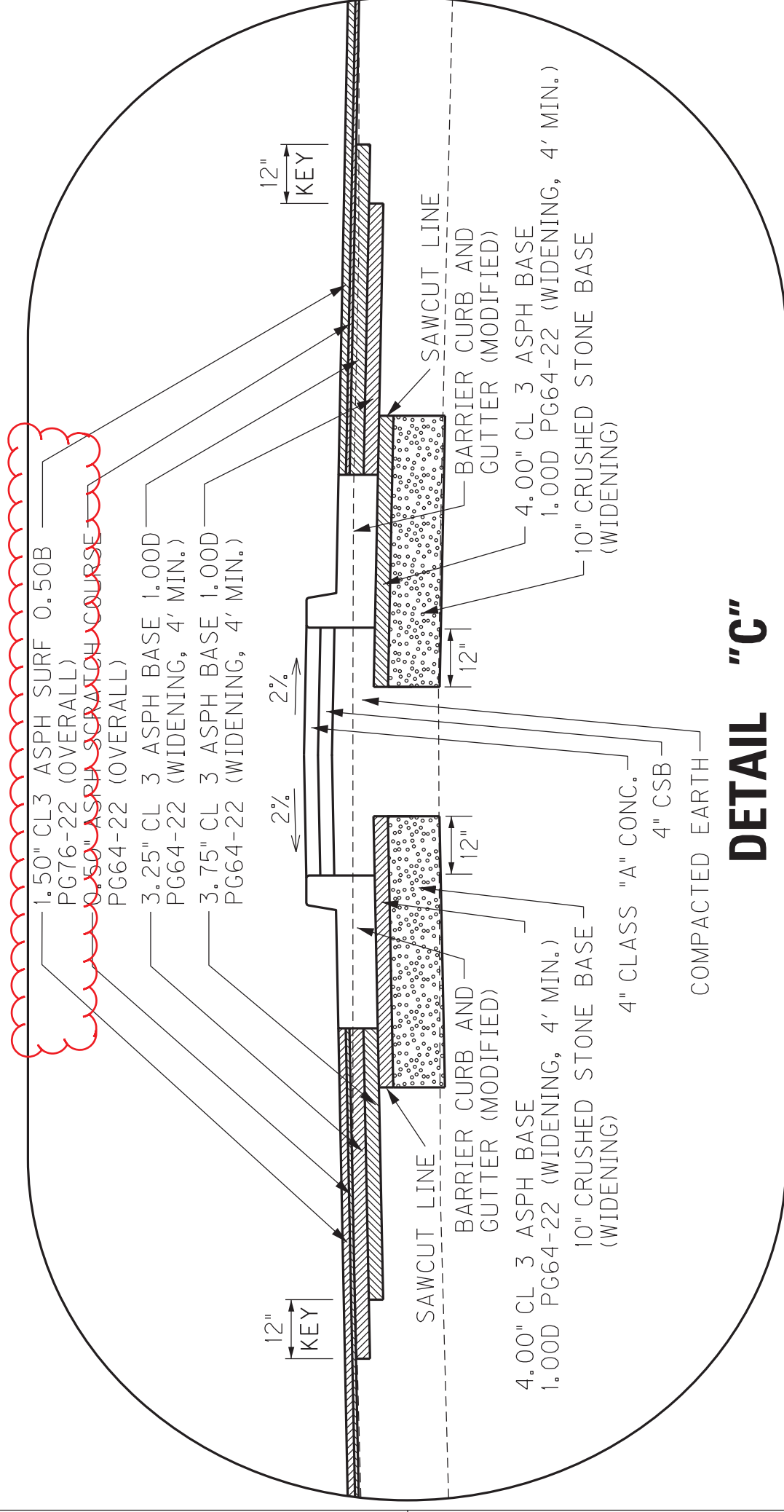
• SEE TYPICAL SECTION NOTES AND PLAN NOTES FOR LOCATIONS OF BARRIER CURB AND GUTTER (MODIFIED) AND STANDARD CURB AND GUTTER (MODIFIED)

**DETAIL "A"**

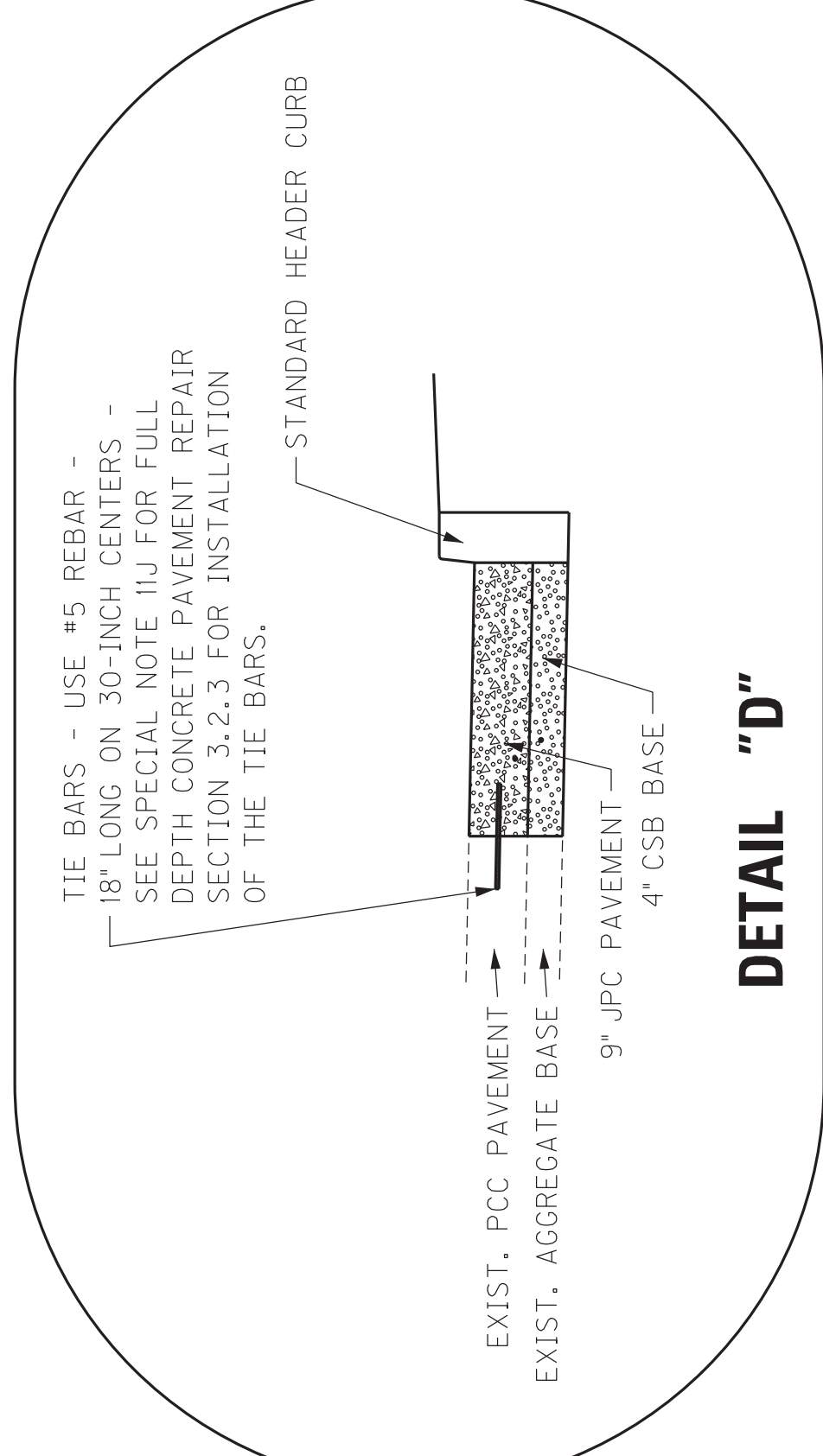


• SEE TYPICAL SECTION NOTES AND PLAN NOTES FOR LOCATIONS OF BARRIER CURB AND GUTTER (MODIFIED) AND STANDARD CURB AND GUTTER (MODIFIED)

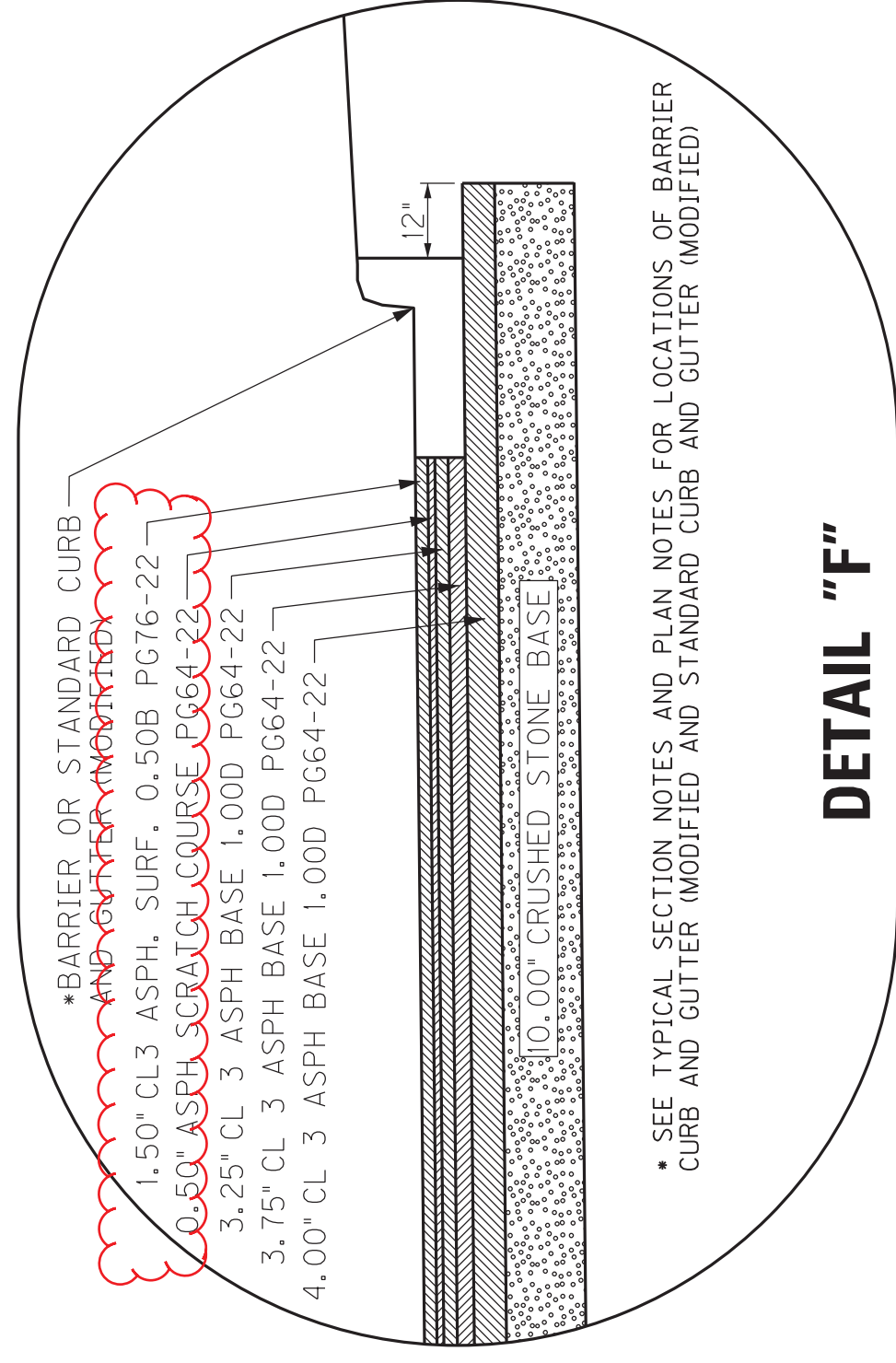
**DETAIL "B"**



**DETAIL "C"**

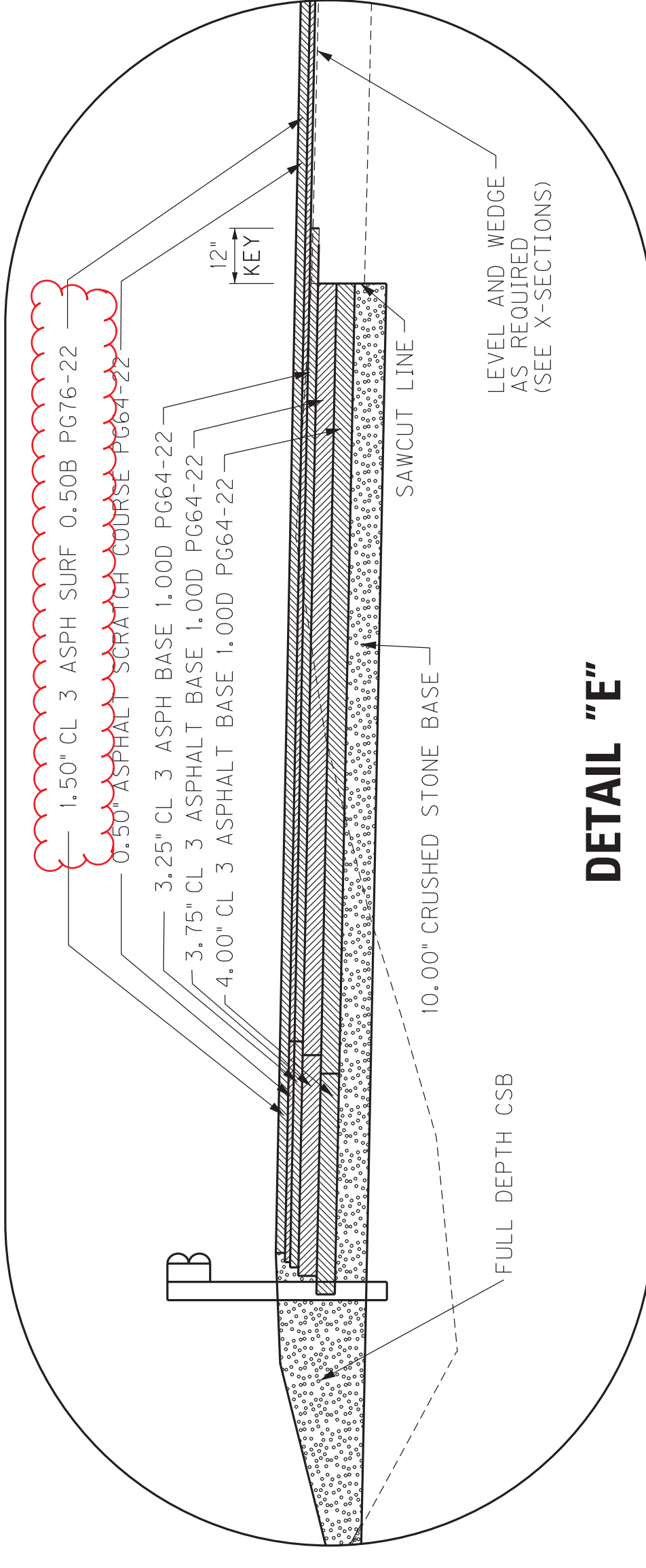


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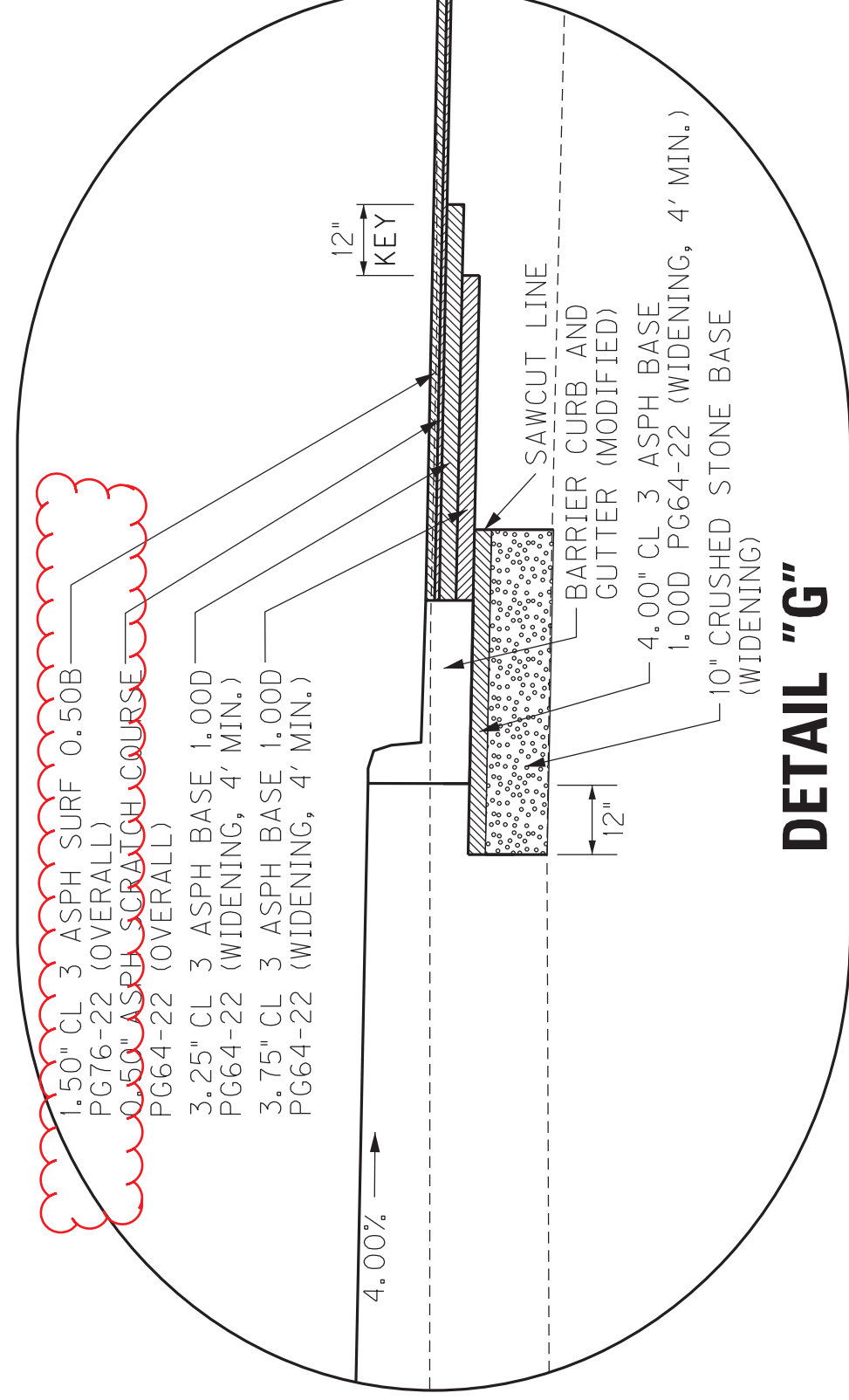


• SEE TYPICAL SECTION NOTES AND PLAN NOTES FOR LOCATIONS OF BARRIER CURB AND GUTTER (MODIFIED) AND STANDARD CURB AND GUTTER (MODIFIED)

**DETAIL "F"**



**DETAIL "E"**

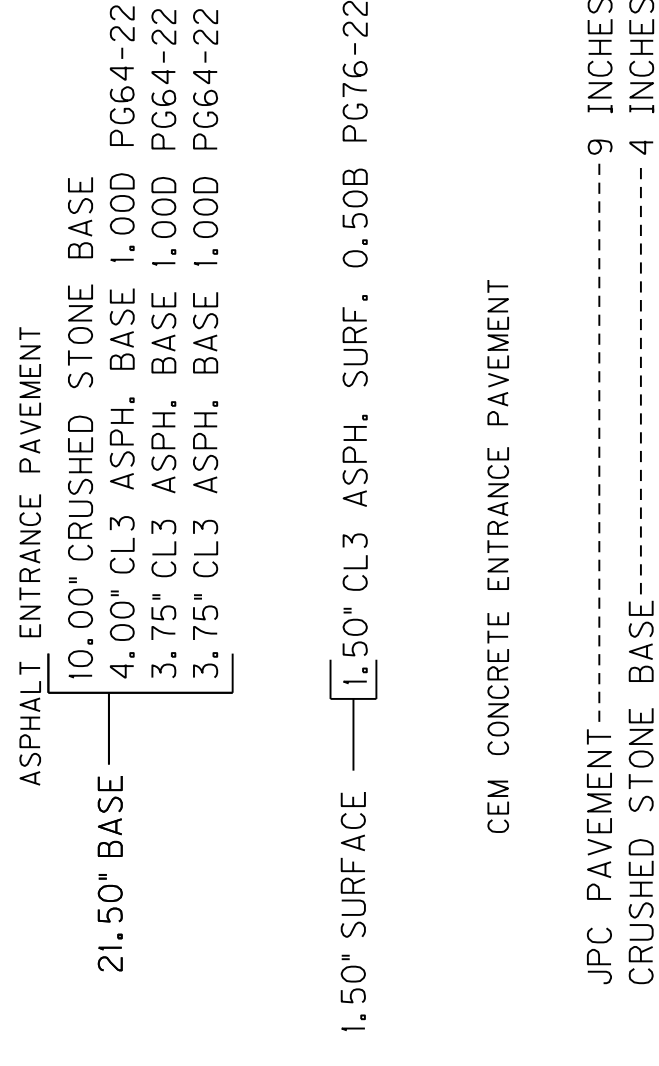
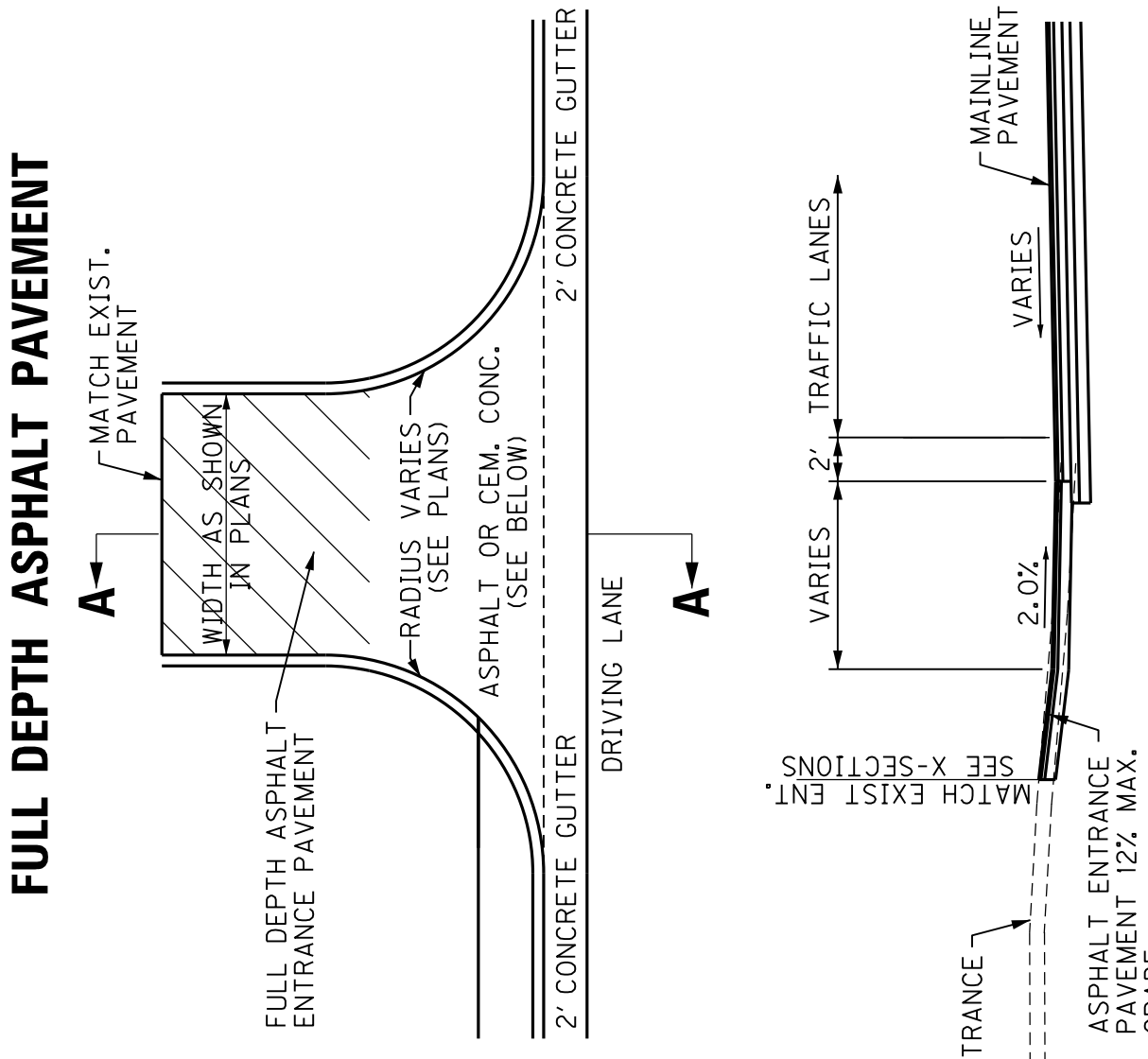


**DETAIL "G"**

# TYPICAL SECTIONS DETAILS

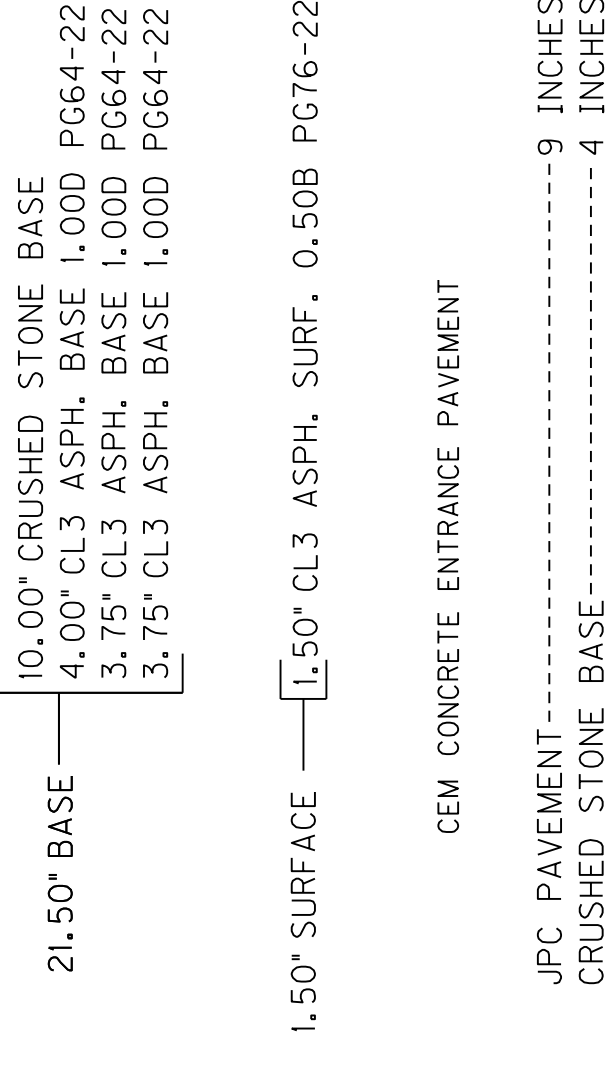
COUNTY OF	ITEM NO.	SHEET NO.
McCRACKEN	1-154.00	R2J

## TYPICAL COMMERCIAL ENTRANCE FULL DEPTH ASPHALT PAVEMENT



### EDGE DRAIN DETAIL

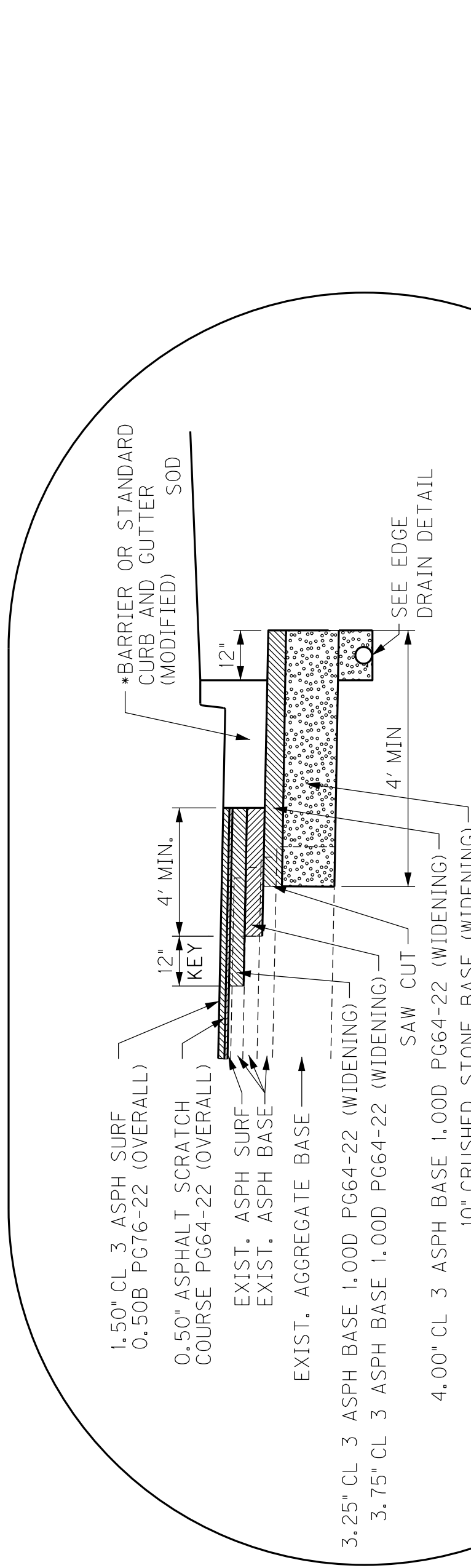
\*THE FOLLOWING ITEMS ARE INCIDENTAL TO EDGE DRAIN INSTALLATION. SEE TYPICAL SECTION FOR LOCATIONS OF BARRIER CURB AND GUTTER (MODIFIED) AND GEOTEXTILE FABRIC FOR WRAPPING PERFORATED PIPE OR FOR WRAPPING COARSE AGGREGATE BACKFILL.



### EDGE KEY DETAIL

WORK UNDER THIS ITEM SHALL INCLUDE CUTTING OUT THE EXISTING BITUMINOUS SURFACE TO A MINIMUM DEPTH AND WIDTH AS SHOWN, SO THE NEW SURFACE MAY HEEL INTO THE EXISTING SURFACE. THE CONTRACT UNIT PRICE BID PER LINEAR FOOT FOR "EDGE KEY" SHALL INCLUDE ALL NECESSARY MATERIALS, LABOR, EQUIPMENT, ETC. TO PERFORM THE WORK AND DISPOSE OF THE BITUMINOUS MATERIAL REMOVED.

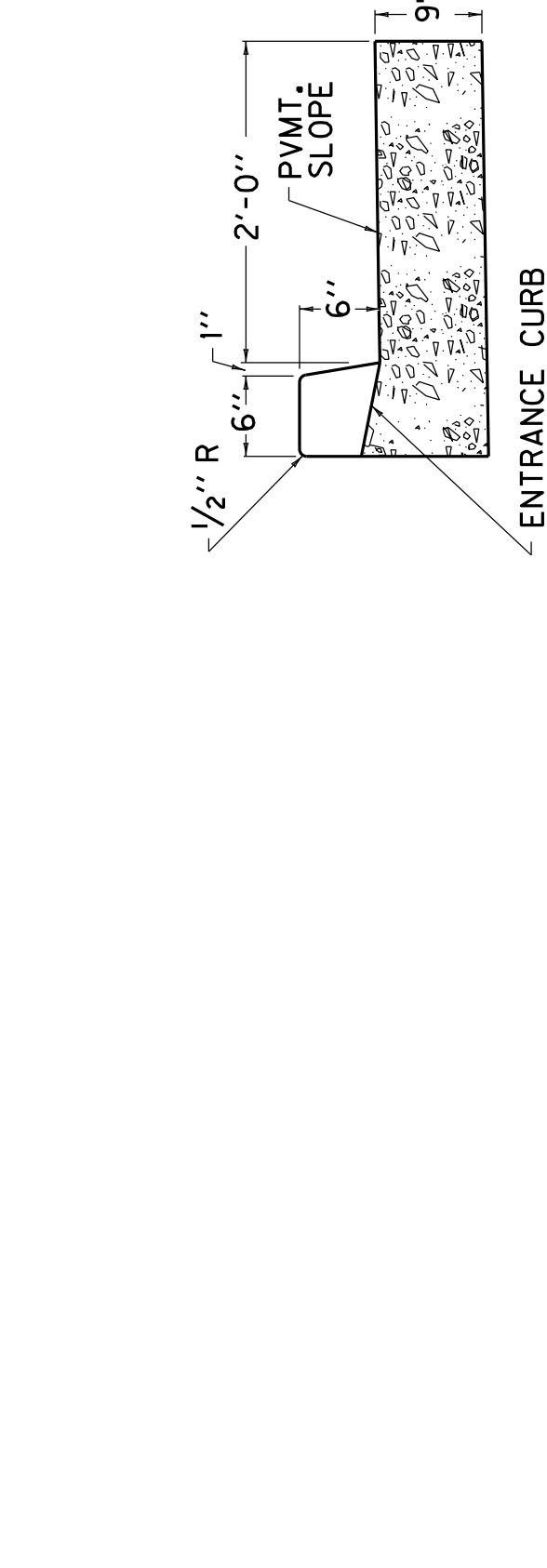
## SECTION A-A ASPHALT ENTRANCE



### DETAIL "H"

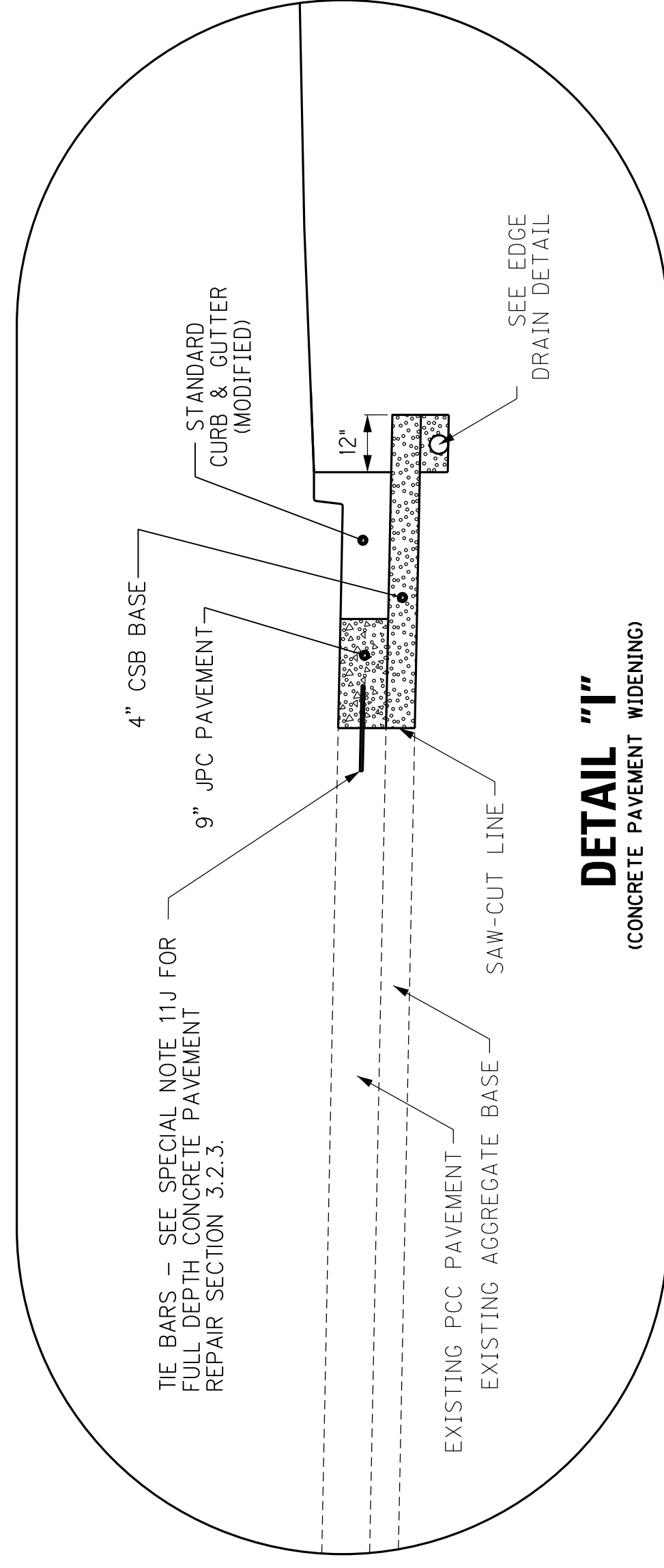
• SEE TYPICAL SECTION NOTES AND PLAN NOTES FOR LOCATIONS OF BARRIER CURB AND GUTTER (MODIFIED) AND STANDARD CURB AND GUTTER (MODIFIED)

## SECTION A-A CEM. CONCRETE ENTRANCE



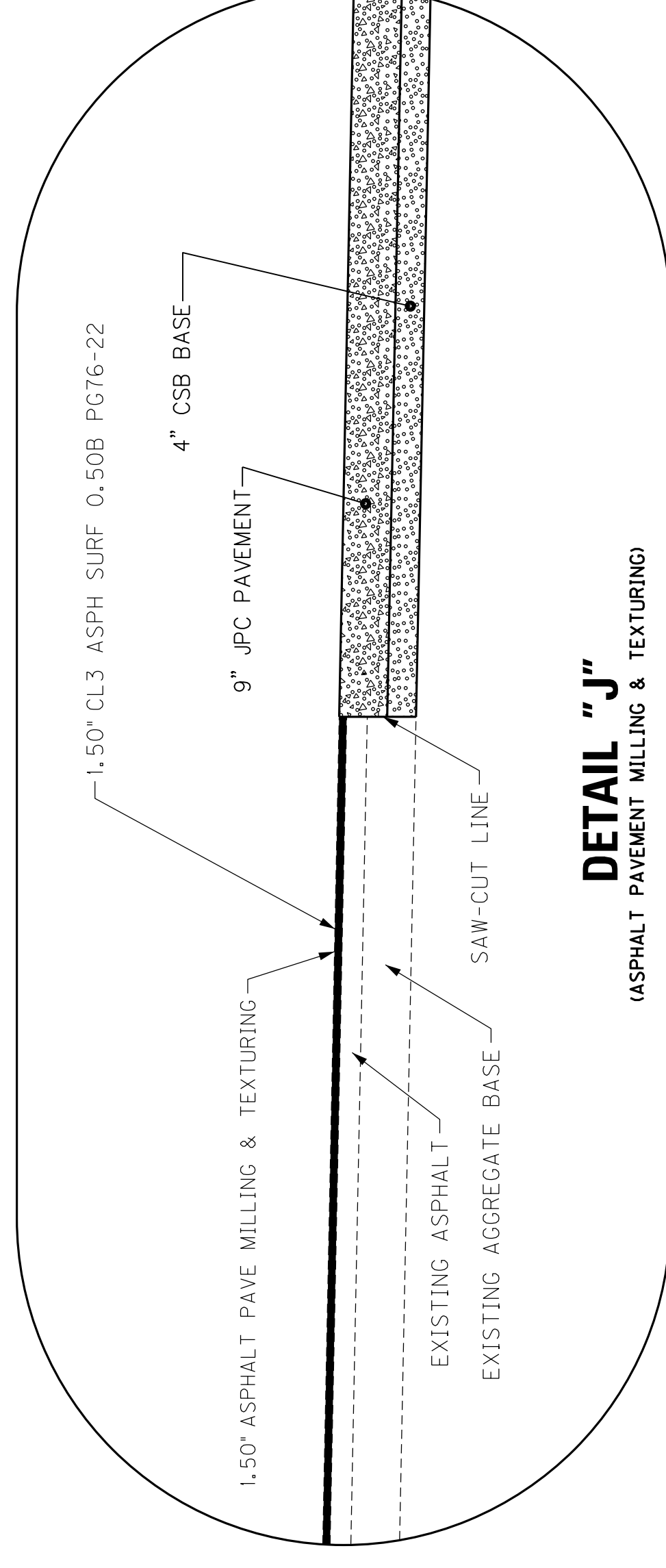
### STANDARD CURB AND GUTTER (MODIFIED)

### BARRIER CURB AND GUTTER (MODIFIED)



### DETAIL "I"

(CONCRETE PAVEMENT WIDENING)



### DETAIL "J"

(ASPHALT PAVEMENT MILLING & TEXTURING)

SCALE: NTS

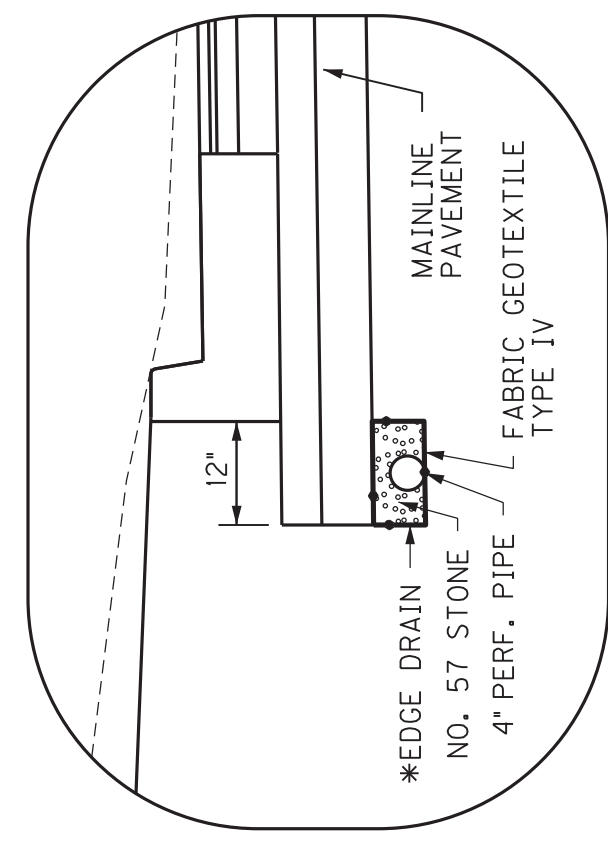
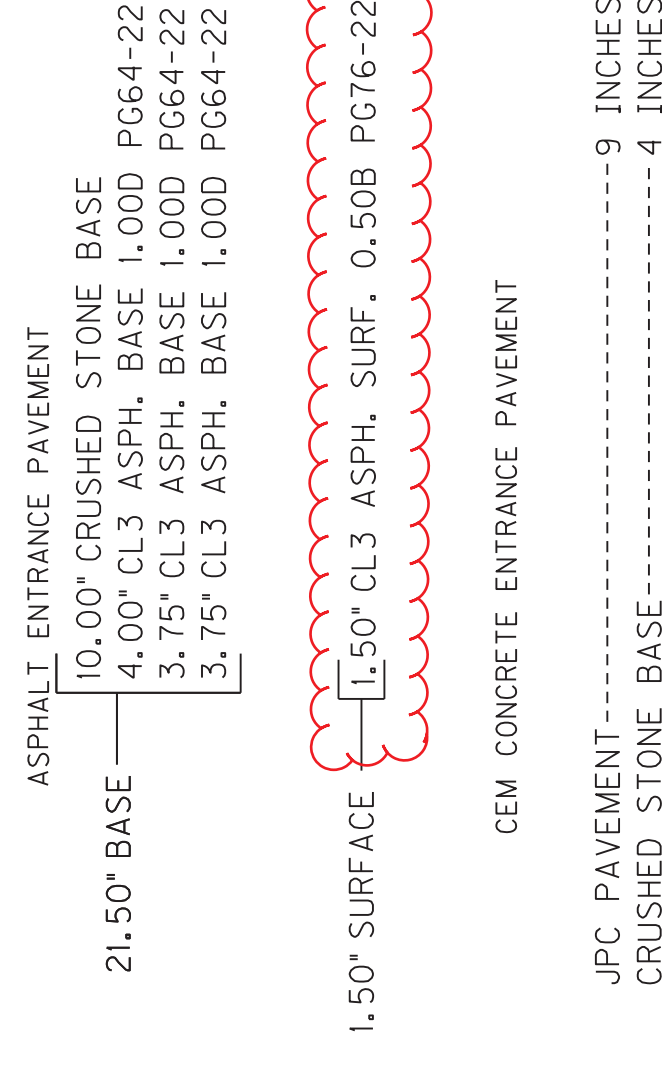
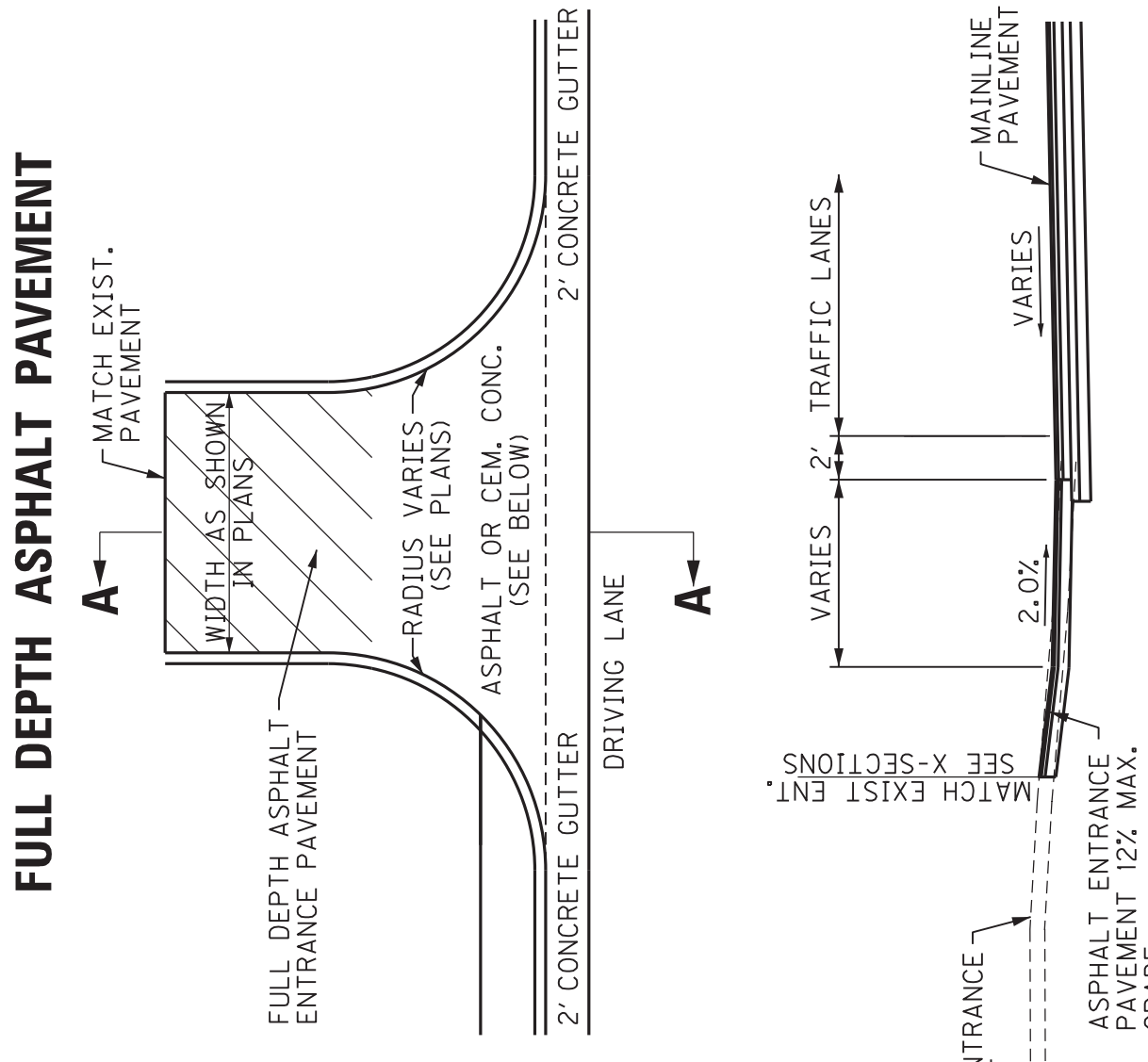
TYPICAL SECTIONS

# TYPICAL SECTIONS DETAILS

COUNTY OF	ITEM NO.	SHEET NO.
McCRACKEN	I-154.00	R2J

REVISED 11-30-18

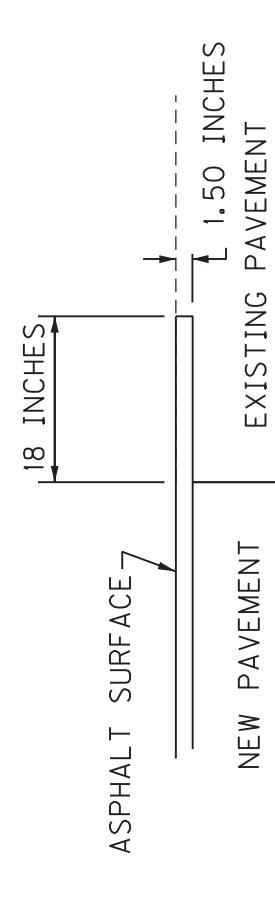
## TYPICAL COMMERCIAL ENTRANCE FULL DEPTH ASPHALT PAVEMENT



### EDGE DRAIN DETAIL

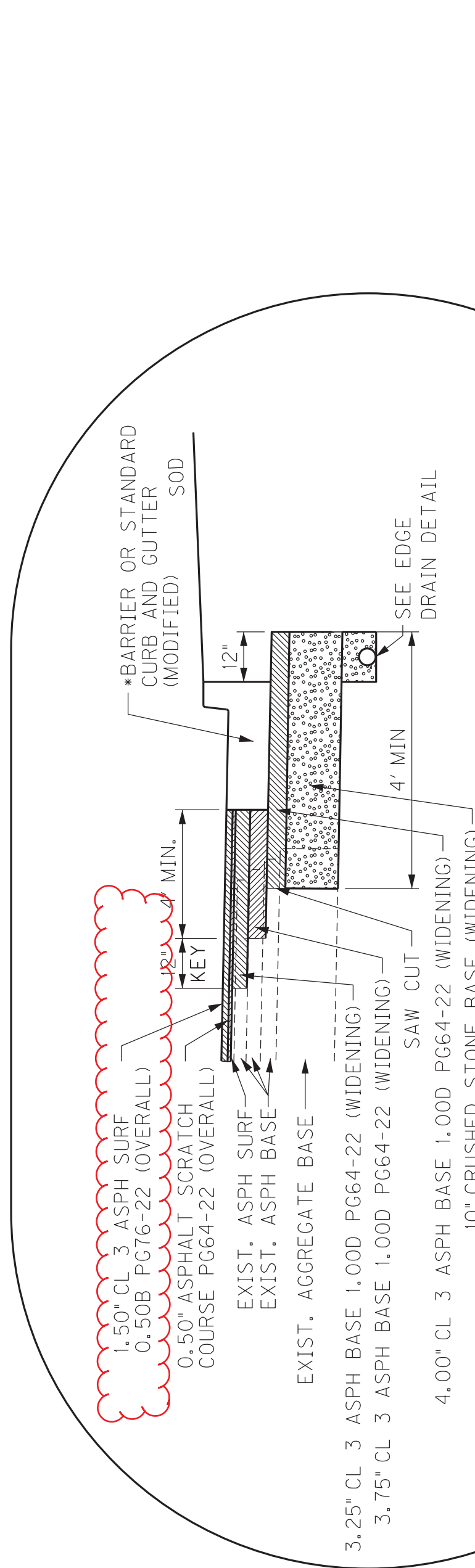
\*THE FOLLOWING ITEMS ARE INCIDENTAL TO EDGE DRAIN INSTALLATION. SEE TYPICAL SECTION FOR TRENCH EXCAVATION, CURB, FCS BEDDING AND BACKFILL, AND GEOTEXTILE FABRIC FOR WRAPPING PERFORATED PIPE OR FOR WRAPPING COARSE AGGREGATE BACKFILL.

### EDGE KEY DETAIL



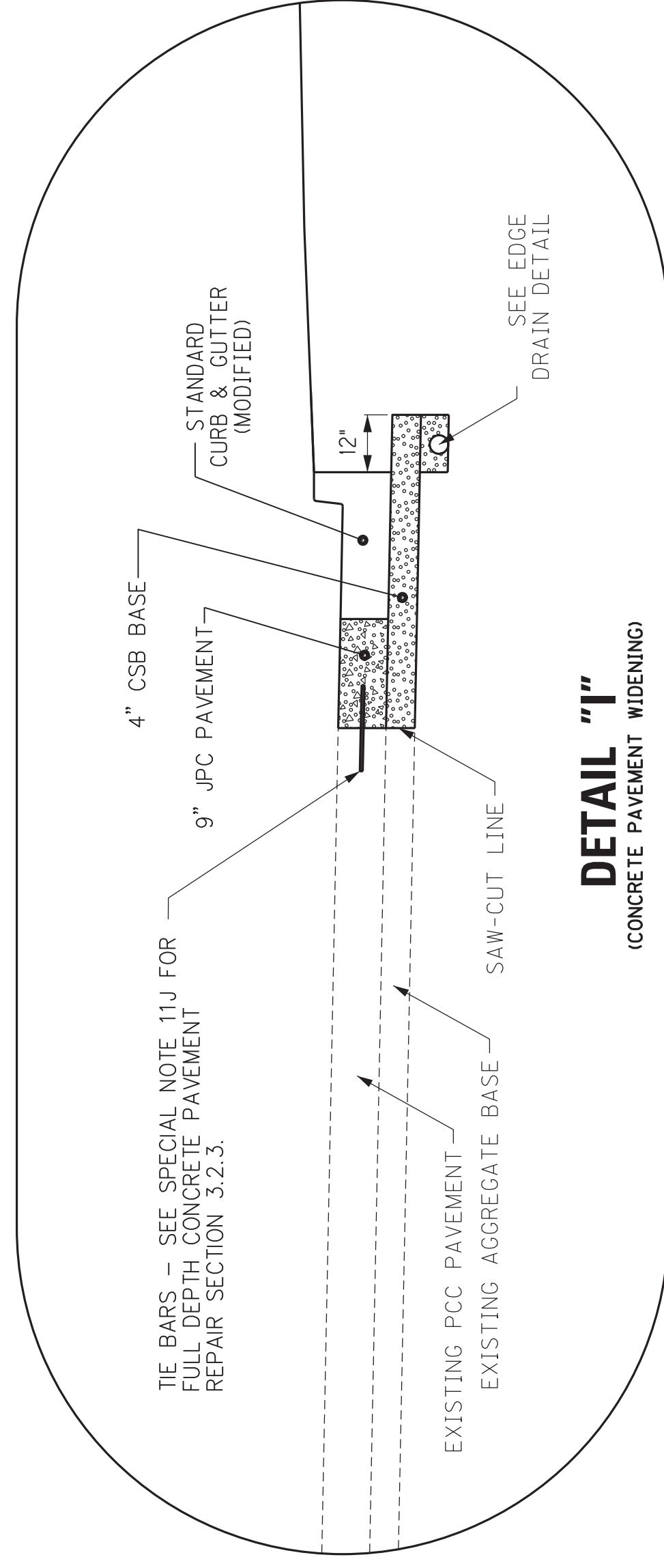
WORK UNDER THIS ITEM SHALL INCLUDE CUTTING OUT THE EXISTING BITUMINOUS SURFACE TO A MINIMUM DEPTH AND WIDTH AS SHOWN, SO THE NEW SURFACE MAY HEEL INTO THE EXISTING SURFACE. THE CONTRACT UNIT PRICE BID PER LINEAR FOOT FOR "EDGE KEY" SHALL INCLUDE ALL NECESSARY MATERIALS, LABOR, EQUIPMENT, ETC. TO REMOVE THE WORK AND DISPOSE OF THE BITUMINOUS MATERIAL REMOVED.

## SECTION A-A ASPHALT ENTRANCE



### DETAIL "H"

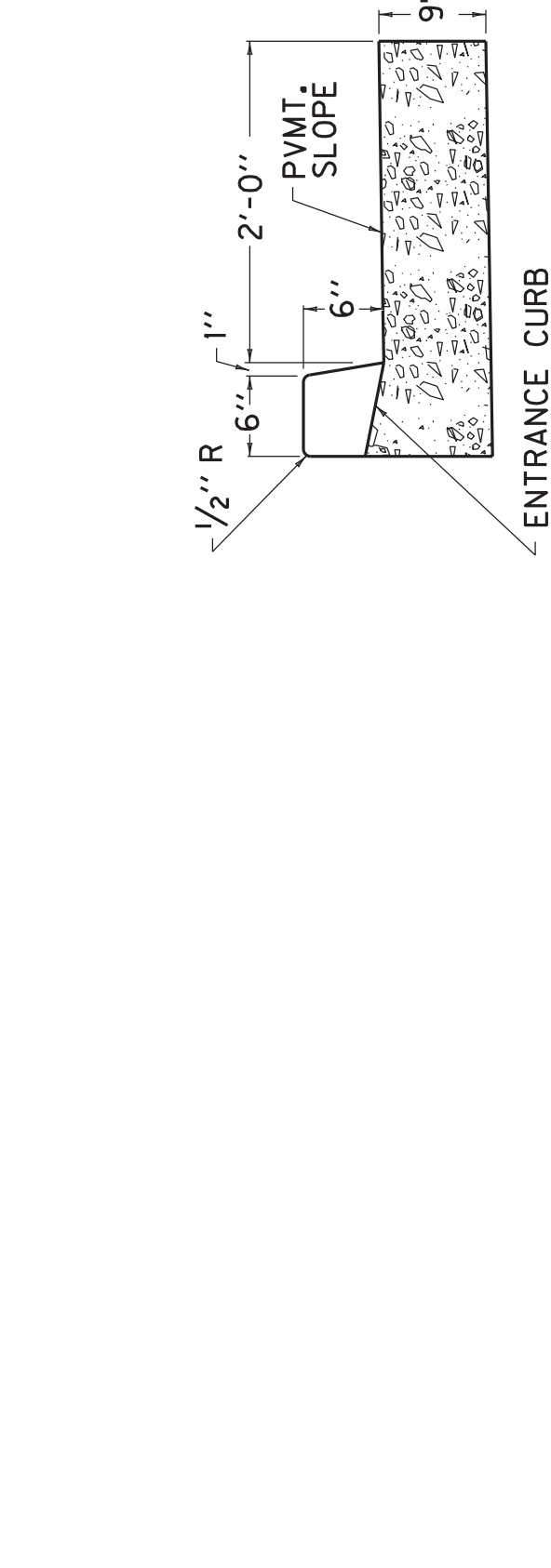
\* SEE TYPICAL SECTION NOTES AND PLAN NOTES FOR LOCATIONS OF BARRIER CURB AND GUTTER (MODIFIED) AND STANDARD CURB AND GUTTER (MODIFIED)



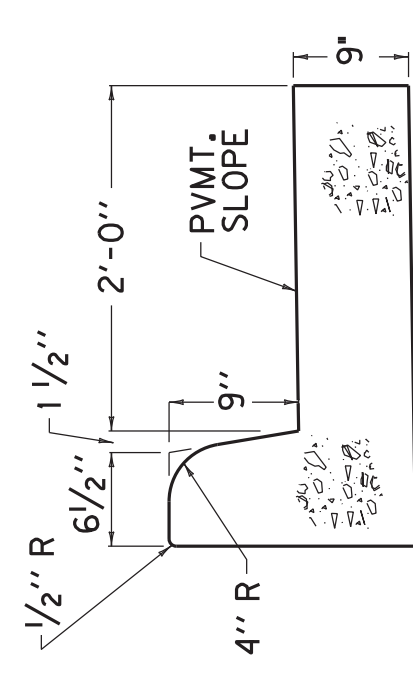
### DETAIL "I"

(CONCRETE PAVEMENT WIDENING)

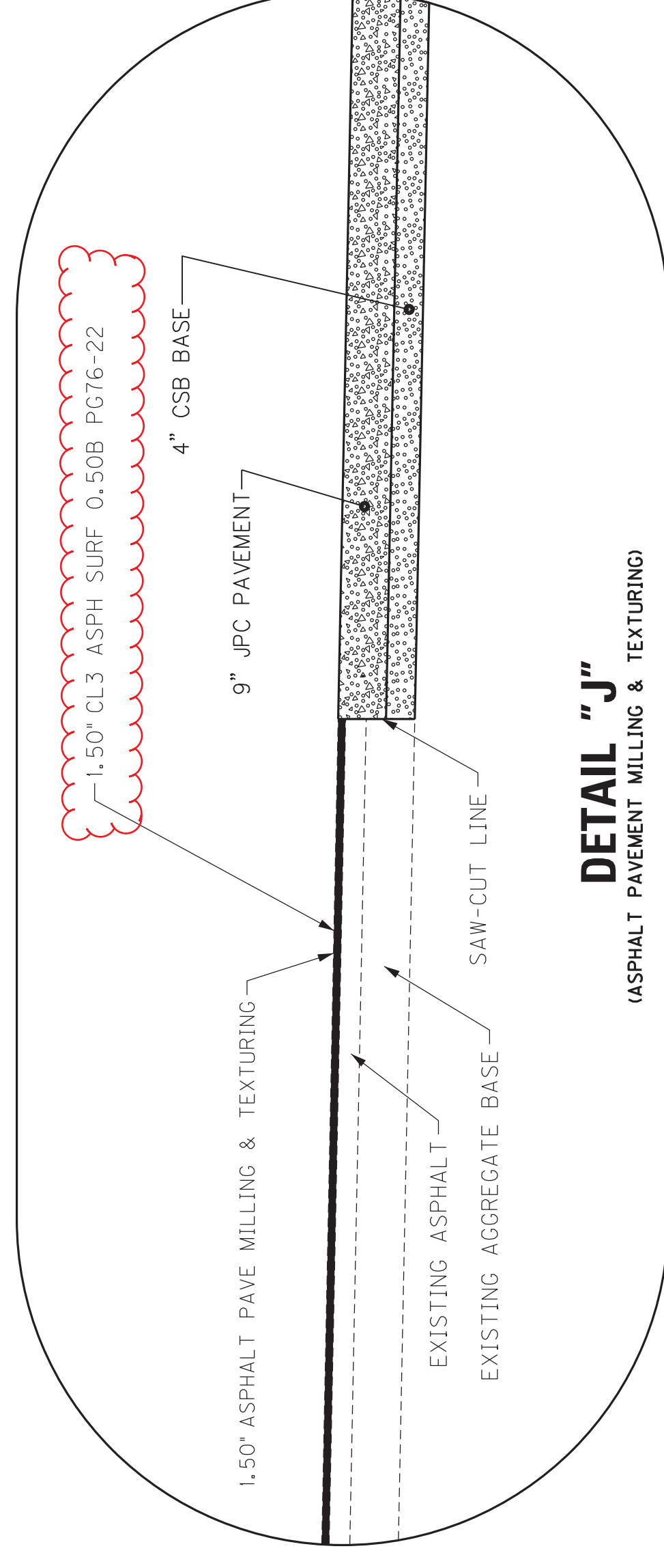
## SECTION A-A CEM. CONCRETE ENTRANCE



### STANDARD CURB AND GUTTER (MODIFIED)



### BARRIER CURB AND GUTTER (MODIFIED)



### DETAIL "J"

(ASPHALT PAVEMENT MILLING & TEXTURING)

# GENERAL SUMMARY

ITEM CODE	ITEM	UNIT	US 60	JAMES SANDERS BLVD	RAMPS A-1 & A-2	RAMPS B & B-1	RAMPS C, C-1 & C-2	RAMPS D & D-1	TOTAL PROJECT
78	CRUSHED AGGREGATE SIZE NO 2 (1)	TON							1,750
1690	FLUME INLET TY 1	EACH	2						2
1691	FLUME INLET TY 2	EACH	3					1	4
1791	ADJUST MANHOLE FRAME TO GRADE	EACH							(2) 7
1792	ADJUST MANHOLE	EACH							2
1811	STANDARD CURB AND GUTTER MOD	LF	7,079		237	358	201	253	8,128
1875	STANDARD HEADER CURB	LF	123	181					304
1919	STANDARD BARRIER MEDIAN TYPE 3	SO YD	171	409					580
1921	STANDARD BARRIER MEDIAN TYPE 4	SO YD	1,118						1,118
1982	DELINEATOR FOR GUARDRAIL M/W	EACH		7			2		9
1983	DELINEATOR FOR GUARDRAIL M/Y	EACH		4			4		8
2003	RELOCATE TEMP CONC BARRIER (3)	LF							2,000
2012	BARRICADE-TYPE I (1)(2)	EACH							9
2014	BARRICADE-TYPE III (1)(2)	EACH							7
2091	REMOVE PAVEMENT (ALL DEPTHS)	SO YD	235						235
2159	TEMP DITCH	LF		4,605					4,605
2160	CLEAN TEMP DITCH	LF		13,815					13,815
2230	EMBANKMENT IN PLACE (A)	CU YD							14,985
2237	DITCHING (4)	LF	154			383			537
2242	WATER (8)	MGAL							87
2351	GUARDRAIL-STEEL W BEAM-S FACE	LF		525			275		800
2367	GUARDRAIL END TREATMENT TYPE 1	EACH		1					1
2369	GUARDRAIL END TREATMENT TYPE 2A	EACH		2			2		4
2381	REMOVE GUARDRAIL (5)	LF		337.5			200	50	537.5
2383	REMOVE & RESET GUARDRAIL	LF							50
2483	CHANNEL LINING CLASS II	TON							284
2545	CLEARING AND GRUBBING (7)	LS							1
2555	CONCRETE-CLASS B (FOR GRAVITY RETAINING WALLS)	CU YD	30						30
2562	TEMPORARY SIGNS (1)(2)	SOFT							414
2568	MOBILIZATION	LS							1
2569	DEMOBILIZATION	LS							1
2585	EDGE KEY	LF	267						267
2587	HOOK BOLT WITH EXPAN ANCHOR	EACH	110						110
2599	FABRIC-GEOTEXTILE TYPE IV (1)	SO YD							2,300
2600	FABRIC GEOTEXTILE TY IV FOR PIPE	SO YD							12,965
2613	HANDRAIL-TYPE A-3	LF	73						73
2650	MAINTAIN AND CONTROL TRAFFIC	LS							1
2671	PORTABLE CHANGEABLE MESSAGE SIGN (1)(2)	EACH							4
2676	MOBILIZATION FOR MILL & TEXT	LS							1
2696	SHOULDER RUMBLE STRIPS-SAWED	LF		1,093			822	450	2,728
2701	TEMP SILT FENCE	LF							4,605
2703	SILT TRAP TYPE A	EACH							14
2704	SILT TRAP TYPE B	EACH							14
2705	SILT TRAP TYPE C	EACH							14
2706	CLEAN SILT TRAP TYPE A	EACH							84
2707	CLEAN SILT TRAP TYPE B	EACH							84
2708	CLEAN SILT TRAP TYPE C	EACH							84
2720	SIDEWALK-4 INCH CONCRETE	SO YD	297						297
2726	STAKING	LS							1
2775	ARROW PANEL (1)(2)	EACH							4
2898	RELOCATE CRASH CUSHION (3)	EACH							4
2998	MASONRY COATING	SO YD	70						70
3171	CONCRETE BARRIER WALL TYPE 9T (3)	LF							2,000
5950	EROSION CONTROL BLANKET	SO YD							3,788
5952	TEMP MULCH	SO YD							69,843
5953	TEMP SEEDING AND PROTECTION	SO YD							6,984
5963	INITIAL FERTILIZER	TON							2.4
5964	20-10-10 FERTILIZER	TON							4
5985	SEEDING AND PROTECTION	SO YD							69,843
5990	SODDING	SO YD	7,274	183	295	208	182		8,142
5992	AGRICULTURAL LIMESTONE	TON							48
6401	FLEXIBLE DELINEATOR POST-M/W	EACH							(9) 75
6404	FLEXIBLE DELINEATOR POST-M/Y	EACH							(9) 75

ITEM CODE	ITEM	UNIT	US 60	JAMES SANDERS BLVD	RAMPS A, A-1 & A-2	RAMPS B & B-1	RAMPS C, C-1 & C-2	RAMPS D & D-1	TOTAL PROJECT
6510	PAVE STRIPING-TEMP PAINT-4 IN (1)(6)	LF	66,940						66,940
6511	PAVE STRIPING-TEMP PAINT-6 IN (1)(6)	LF		2,568	4,964	1,496	4,090	902	14,020
6514	PAVE STRIPING-PERM PAINT-4 IN	LF	23,804	9,711					33,515
6515	PAVE STRIPING-PERM PAINT-6 IN	LF	167		2,338	1,110	2,192	1,102	6,909
6516	PAVE STRIPING-PERM PAINT-8 IN	LF	1,376	1,464					2,840
6517	PAVE STRIPING-PERM PAINT 12 IN	LF			160				320
6566	PAVE MARKING-THERMO X-WALK-12 IN	LF			350	122			472
6568	PAVE MARKING-THERMO STOP BAR-24IN	LF	647	115	77				908
6570	PAVE MARKING-PAINT CROSS-HATCH	SO FT	912	551					1,463
6572	PAVE MARKING-DOTTED LANE EXTEN	LF	683						683
6573	PAVE MARKING-THERMO STR ARROW	EACH	47	2					49
6574	PAVE MARKING-THERMO CURV ARROW	EACH	48	16	14				100
6575	PAVE MARKING-THERMO COMB ARROW	EACH	30	4	1				36
6576	PAVE MARKING-THERMO ONLY	EACH	4	3					7
6583	PAVEMENT MARKER TYPE IV-B W/R	EACH	85						85
6584	PAVEMENT MARKER TYPE IV-B Y/R	EACH	142		9	11	7	12	181
6600	REMOVE PAVEMENT MARKER TYPE V	EACH							544
8001	STRUCTURE EXCAVATION-COMMON	CU YD	37						37
8100	CONCRETE CLASS A (FOR MEDIANS)	CU YD	364						364
8100	CONCRETE-CLASS A (FOR CONCRETE MEDIAN BARRIER END COND. NO. 2)	CU YD	8.14						8.14
8100	CONCRETE-CLASS A (FOR CRASH WALL)	CU YD	42.50						42.50
8150	STEEL REINFORCEMENT	LB							(10) 509
8151	STEEL REINFORCEMENT-EPOXY COATED	LB	2,022						2,022
8902	CRASH CUSHION TY VI CLASS B TL3	EACH	2						2
8903	CRASH CUSHION TY VI CLASS BT TL3 (3)	EACH							4
10020NS	FUEL ADJUSTMENT	DOLL							39,467
10030NS	ASPHALT ADJUSTMENT	DOLL							78,215
20071EC	JOINT ADHESIVE	LF							38,671
20099ES842	PAVE MARK TEMP PAINT STOP BAR (1)	LF							264
20191ED	OBJECT MARKER TY 3	EACH							1
20430ED	SAW CUT (3)	LF	13,336		650	577	970	20	15,553
22520EN	PAVE MARKING-THERMO YIELD BAR-36 IN	LF				18			36
22664EN	WATER BLASTING EXISTING STRIPE	LF							24,000
22861EN	HIGH STRENGTH GEOTEXTILE FABRIC TY V (1)	SO YD							1,200
23143ED	KPDES PERMIT AND TEMP EROSION CONTROL	LS							1
23158ES505	DETECTABLE WARNINGS	SO FT	194			40			274
23274EN1F	TURF REINFORCEMENT MAT 1	SO YD							846
23791EC	PAVE STRIPING-CHEVRON MARKINGS	SO FT	95	250					345
24109EC	BARRIER CURB AND GUTTER-MOD	LF	4,388		297	419	254	354	5,712
24110EC	PERM PAINT-BARRIER CURB	LF	4,366		280	409	240	338	5,633
24489EC	INLAID PAVEMENT MARKER	EACH	614	124	64	29	65	25	921
24631EC	BARCODE SIGN INVENTORY	EACH							(9) 105
24640ED	OBJECT MARKER TYPE 1	EACH	2						2
24814EC	PIPELINE INSPECTION	LF							4,024

**NOTES:**

- (1) FOR MAINTENANCE OF TRAFFIC.
- (2) THE QUANTITIES FOR THESE ITEMS INCLUDE INITIAL PLACEMENT ONLY. ANY RELOCATION REQUIRED WILL NOT BE PAID FOR DIRECTLY BUT WILL BE CONSIDERED INCIDENTAL.
- (3) FOR FULL-DEPTH PAVEMENT ADJACENT TO EXISTING PAVEMENT.
- (4) DITCHING INTENDED FOR CLEANING AND REGRADING OF ROADSIDE DITCHES. REPAIR OF PIPE OUTLET DITCHES, AND CLEANING OF DRAINAGE STRUCTURES TO RESTORE POSITIVE DRAINAGE. ANY EMBANKMENT AND REGRADING REQUIRED IS INCIDENTAL TO DITCHING.
- (5) DISMANTLE, SALVAGE, AND STOCKPILE AS DIRECTED BY THE ENGINEER. SALVAGED ITEMS SHALL BECOME THE PROPERTY OF THE DEPARTMENT OF HIGHWAYS.
- (6) TEMPORARY STRIPING TAPE MAY BE USED INSTEAD OF TEMPORARY PAINT, BUT WILL BE PAID AT THE CONTRACT UNIT PRICE FOR TEMPORARY PAINT.
- (7) APPROXIMATELY 14.37 ACRES
- (8) FOR CONTROLLING DUST CAUSED BY MAINTAINING TRAFFIC ONLY
- (9) TOTAL CARRIED OVER FROM SIGNING QUANTITIES
- (10) TOTAL INCLUDES 183 LBS CARRIED OVER FROM PIPE DRAINAGE SUMMARY
- (11) LOCATIONS ARE NOT NOTED IN THE PLANS, TO BE USED AS DIRECTED BY ENGINEER
- (12) TOTAL INCLUDES 3 STORM SEWER MANHOLES CARRIED OVER FROM PIPE DRAINAGE SUMMARY
- (13) USAGE AND PLACEMENT WILL BE AT THE DISCRETION OF THE ENGINEER

(A) EARTHWORK TOTALS  
 EMBANKMENT ----- 14,985 CU YD  
 ROADWAY EXCAVATION ----- 12,931 CU YD

NOTE:  
 ESTIMATE FOR EARTHWORK CALCULATIONS FOR DESIGN ONLY. THE CONTRACTOR IS ADVISED THAT THE EARTHWORK CALCULATIONS SHOWN ARE FOR INFORMATION ONLY. ASSUMPTIONS FOR SHRINKAGE AND SWELL FACTORS ARE THE CONTRACTOR'S RESPONSIBILITY.

## GENERAL SUMMARY



# GENERAL SUMMARY

ITEM CODE	ITEM	UNIT	US 60	JAMES SANDERS BLVD	RAMPS A-1 & A-2	RAMPS B & B-1	RAMPS C, C-1 & C-2	RAMPS D & D-1	TOTAL PROJECT
78	CRUSHED AGGREGATE SIZE NO 2 (1)	TON	1,750						1,750
1690	FLUME INLET TY 1	EACH	2						2
1691	FLUME INLET TY 2	EACH	3						4
1791	ADJUST MANHOLE FRAME TO GRADE	EACH	(2)						7
1792	ADJUST MANHOLE	EACH							2
1811	STANDARD CURB AND GUTTER MOD	LF	7,079		237	358	201	253	8,128
1875	STANDARD HEADER CURB	LF	123	181					304
1919	STANDARD BARRIER MEDIAN TYPE 3	SO YD	171	409					580
1921	STANDARD BARRIER MEDIAN TYPE 4	SO YD	1,118						1,118
1982	DELINEATOR FOR GUARDRAIL M/W	EACH			7		2		9
1983	DELINEATOR FOR GUARDRAIL W/O	EACH			4		4		8
2003	RELOCATE TEMP CONC BARRIER (3)	LF							2,000
2012	BARRICADE TYPE I (1)(2)	EACH							9
2014	BARRICADE TYPE III (1)(2)	EACH							7
2091	REMOVE PAVEMENT (ALL DEPTHS)	SO YD	235						235
2159	TEMP DITCH	LF	4,605						4,605
2160	CLEAN TEMP DITCH	LF	13,815						13,815
2230	EMBANKMENT IN PLACE (A)	CU YD	14,985						14,985
2237	DITCHING (4)	LF	154			383			537
2242	WATER (8)	MGAL							87
2351	GUARDRAIL-STEEL W BEAM-S FACE	LF		525			275		800
2367	GUARDRAIL END TREATMENT TYPE 1	EACH		1					1
2369	GUARDRAIL END TREATMENT TYPE 2A	EACH		2			2		4
2381	REMOVE GUARDRAIL (5)	LF	537.5				200		537.5
2383	REMOVE & RESET GUARDRAIL	LF						50	50
2483	CHANNEL LINING CLASS II	TON	284						284
2545	CLEARING AND GRUBBING (7)	LS							1
2555	CONCRETE-CLASS B (FOR GRAVITY RETAINING WALLS)	CU YD	30						30
2562	TEMPORARY SIGNS (1)(2)	SOFT	414						414
2568	MOBILIZATION	LS	1						1
2569	DEMOBILIZATION	LS	1						1
2585	EDGE KEY	LF	267						267
2587	HOOK BOLT WITH EXPAN ANCHOR	LF	110						110
2599	FABRIC-GEOTEXTILE TYPE IV (1)	SO YD	2,300						2,300
2600	FABRIC GEOTEXTILE TY IV FOR PIPE	SO YD	12,965						12,965
2613	HANDRAIL-TYPE A-3	LF	73						73
2650	MAINTAIN AND CONTROL TRAFFIC	LS	1						1
2671	PORTABLE CHANGEABLE MESSAGE SIGN (1)(2)	EACH	4						4
2676	MOBILIZATION FOR MILL & TEXT	LS	1						1
2696	SHOULDER RUMBLE STRIPS-SAWED	LF	4,605						4,605
2701	TEMP SILT FENCE	LF	14						14
2703	SILT TRAP TYPE A	EACH	14						14
2704	SILT TRAP TYPE B	EACH	14						14
2705	SILT TRAP TYPE C	EACH	14						14
2706	CLEAN SILT TRAP TYPE A	EACH	84						84
2707	CLEAN SILT TRAP TYPE B	EACH	84						84
2708	CLEAN SILT TRAP TYPE C	EACH	84						84
2720	SIDEWALK-4 INCH CONCRETE	SO YD	297						297
2726	STAKING	LS	1						1
2775	ARROW PANEL (1)(2)	EACH	4						4
2898	RELOCATE CRASH CUSHION (3)	EACH	4						4
2998	WALKWAY BARRICADE	SO YD	1,000						1,000
3171	CONCRETE BARRIER WALL TYPE 9T (3)	LF	2,000						2,000
5950	EROSION CONTROL BLANKET	SO YD	3,188						3,188
5952	TEMP MULCH	SO YD	69,843						69,843
5953	TEMP SEEDING AND PROTECTION	SO YD	6,984						6,984
5963	INITIAL FERTILIZER	TON	2.4						2.4
5964	20-10-10 FERTILIZER	TON	4						4
5985	SEEDING AND PROTECTION	SO YD	69,843						69,843
5990	SODDING	SO YD	8,142						8,142
5992	AGRICULTURAL LIMESTONE	TON	48						48
6401	FLEXIBLE DELINEATOR POST-M/W	EACH	(9)						(9)
6404	FLEXIBLE DELINEATOR POST-M/Y	EACH	(9)						(9)

ITEM CODE	ITEM	UNIT	US 60	JAMES SANDERS BLVD	RAMPS A, A-1 & A-2	RAMPS B & B-1	RAMPS C, C-1 & C-2	RAMPS D & D-1	TOTAL PROJECT
6510	PAVE STRIPING-TEMP PAINT-4 IN (1)(6)	LF	66,940						66,940
6511	PAVE STRIPING-TEMP PAINT-6 IN (1)(6)	LF	2,568	2,568	4,964	1,496	4,090	902	14,020
6514	PAVE STRIPING-PERM PAINT-4 IN	LF	23,804	9,711					33,515
6515	PAVE STRIPING-PERM PAINT-6 IN	LF	167		2,338	1,110	2,192	1,102	6,909
6516	PAVE STRIPING-PERM PAINT-8 IN	LF	1,376	1,464					2,840
6517	PAVE STRIPING-PERM PAINT 12 IN	LF			160				320
6566	PAVE MARKING-THERMO X-WALK-12 IN	LF			350	122			472
6568	PAVE MARKING-THERMO STOP BAR-24IN	LF	647	115	77				908
6570	PAVE MARKING-PAINT CROSS-HATCH	SO FT	912	551					1,463
6572	PAVE MARKING-DOTTED LANE EXTEN	LF	683						683
6573	PAVE MARKING-THERMO STR ARROW	EACH	47	2					49
6574	PAVE MARKING-THERMO CURV ARROW	EACH	48	16	14				100
6575	PAVE MARKING-THERMO COMB ARROW	EACH	30	4	1				36
6576	PAVE MARKING-THERMO ONLY	EACH	4	3					7
6583	PAVEMENT MARKER TYPE IV-B W/R	EACH	85						85
6584	PAVEMENT MARKER TYPE IV-B Y/R	EACH	142		9	11	7	12	181
6600	REMOVE PAVEMENT MARKER TYPE V	EACH							544
8001	STRUCTURE EXCAVATION-COMMON	CU YD	37						37
8100	CONCRETE CLASS A (FOR MEDIANS)	CU YD	364						364
8100	CONCRETE-CLASS A (FOR CONCRETE MEDIAN BARRIER END COND. NO. 2)	CU YD	8.14						8.14
8100	CONCRETE-CLASS A (FOR CRASH WALL)	CU YD	42.50						42.50
8150	STEEL REINFORCEMENT	LB							(10) 509
8151	STEEL REINFORCEMENT-EPOXY COATED	LB	2,022						2,022
8902	CRASH CUSHION TY VI CLASS-B-TL3	EACH	2						2
8903	CRASH CUSHION TY VI CLASS BT TL3 (3)	EACH							4
10020NS	FUEL ADJUSTMENT	SOFT							39,467
10030NS	ASPHALT ADJUSTMENT	DOLL							78,215
20071EC	JOINT ADHESIVE	LF							38,671
20099ES842	PAVE MARK TEMP PAINT STOP BAR (1)	LF							264
20191ED	OBJECT MARKER TY 3	EACH			1				1
20430ED	SAW CUT (3)	LF	13,336		650	577	970	20	15,553
22520EN	PAVE MARKING-THERMO YIELD BAR-36 IN	LF			18				36
22664EN	WATER BLASTING EXISTING STRIPE	LF							24,000
22861EN	HIGH STRENGTH GEOTEXTILE FABRIC TY V (1)	SO YD							1,200
23143ED	KPDES PERMIT AND TEMP EROSION CONTROL	LS							1
23158ES505	DETECTABLE WARNINGS	SO FT	194		40				274
23274EN1F	TURF REINFORCEMENT MAT 1	SO YD	846						846
23791EC	PAVE STRIPING-CHEVRON MARKINGS	SO FT	95	250					345
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24110EC	PERM PAINT-BARRIER CURB	LF	4,366		280	409	240	338	5,633
24489EC	INLAID PAVEMENT MARKER	EACH	614	124	64	29	65	25	921
24631EC	BARCODE SIGN INVENTORY	EACH							(9) 105
24640ED	OBJECT MARKER TYPE 1	EACH	2						2
24814EC	PIPELINE INSPECTION	LF							4,024

**NOTES:**

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- THE QUANTITIES FOR THESE ITEMS INCLUDE INITIAL PLACEMENT ONLY. ANY RELOCATION REQUIRED WILL NOT BE PAID FOR DIRECTLY BUT WILL BE CONSIDERED INCIDENTAL.
- FOR FULL-DEPTH PAVEMENT ADJACENT TO EXISTING PAVEMENT.
- DITCHING INTENDED FOR CLEANING AND REGRADING OF ROADSIDE DITCHES. REPAIR OF PIPE OUTLET DITCHES, AND CLEANING OF DRAINAGE STRUCTURES TO RESTORE POSITIVE DRAINAGE. ANY EMBANKMENT AND REGRADING REQUIRED IS INCIDENTAL TO DITCHING.
- DISMANTLE, SALVAGE, AND STOCKPILE AS DIRECTED BY THE ENGINEER. SALVAGED ITEMS SHALL BECOME THE PROPERTY OF THE DEPARTMENT OF HIGHWAYS.
- TEMPORARY STRIPING TAPE MAY BE USED INSTEAD OF TEMPORARY PAINT, BUT WILL BE PAID AT THE CONTRACT UNIT PRICE FOR TEMPORARY PAINT.
- APPROXIMATELY 14.37 ACRES
- FOR CONTROLLING DUST CAUSED BY MAINTAINING TRAFFIC ONLY
- TOTAL CARRIED OVER FROM SIGNING QUANTITIES
- TOTAL INCLUDES 183 LBS CARRIED OVER FROM PIPE DRAINAGE SUMMARY
- LOCATIONS ARE NOT NOTED IN THE PLANS, TO BE USED AS DIRECTED BY ENGINEER
- TOTAL INCLUDES 3 STORM SEWER MANHOLES CARRIED OVER FROM PIPE DRAINAGE SUMMARY
- USAGE AND PLACEMENT WILL BE AT THE DISCRETION OF THE ENGINEER

(A) EARTHWORK TOTALS

EMBANKMENT ----- 14,985 CU YD  
 ROADWAY EXCAVATION ----- 12,931 CU YD

NOTE:  
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## GENERAL SUMMARY













# MAINTENANCE OF TRAFFIC NOTES

COUNTY OF	ITEM NO.	SHEET NO.
McCRACKEN	1-154.00	R69

## GENERAL TRAFFIC CONTROL NOTES

- TRAFFIC SHALL BE MAINTAINED IN ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD), THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND THE STANDARD DRAWINGS, CURRENT EDITIONS.
- ALL TEMPORARY TRAFFIC CONTROL DEVICES SHALL COMPLY WITH THE NATIONAL COOPERATIVE HIGHWAY RESEARCH PROGRAM (NCHRP) 350 REPORT.
- LANE CLOSURES SHALL NOT BE ALLOWED DURING THE HOURS OF 6:00 AM TO 7:00 PM MONDAY THROUGH FRIDAY. FURTHER, THEY SHALL NOT BE ALLOWED DURING ALL HOLIDAYS IDENTIFIED IN SECTION 101 OF THE STANDARD SPECIFICATIONS (INDEPENDENCE DAY, LABOR DAY & VETERAN'S DAY) OR SPECIAL EVENTS INCLUDING THE AOS PADUCAH QUILT SHOW.  
LISTED BELOW ARE DATES AND TIME FOR HOLIDAYS WHEN LANE CLOSURES WILL NOT BE ALLOWED.  
EASTER 6:00 AM THUR TO 9:00 PM MON  
MEMORIAL DAY 6:00 AM THUR TO 9:00 PM TUES  
JULY 4TH 6:00 AM JULY 1ST TO 9:00 PM JULY 7TH  
LABOR DAY 6:00 AM THUR TO 9:00 PM TUES
- THE PROPOSED DRAINAGE INLETS ON THIS PROJECT SHALL HAVE WEEP HOLES, BLEEDERS OR ANOTHER ACCEPTABLE FORM OF TEMPORARY DRAINAGE INSTALLED TO ALLOW FOR POSITIVE DRAINAGE DURING CONSTRUCTION PHASING.
- EXCEPT FOR THE ROADWAY AND TRAFFIC CONTROL BID ITEMS LISTED, ALL ITEMS OF WORK NECESSARY TO MAINTAIN AND CONTROL TRAFFIC WILL BE PAID FOR AT THE LUMP SUM PRICE TO MAINTAIN AND CONTROL TRAFFIC AS SET FORTH IN THE CURRENT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION UNLESS OTHERWISE PROVIDED FOR IN THESE NOTES BUT IS NOT LIMITED TO THE FOLLOWING ITEMS AND OPERATIONS:
  - ALL GRADING AND NECESSARY DRAINAGE (UNLESS A BID ITEM FOR DETOUR CONSTRUCTION IS INCLUDED) FOR THE TEMPORARY ROADWAY AND REMOVAL THEREOF, WHEN IT IS NO LONGER REQUIRED, IF A BID ITEM FOR DETOUR CONSTRUCTION IS INCLUDED, GRADING AND DRAINAGE WILL BE PAID FOR IN THE BID ITEM "DETOUR CONSTRUCTION".
  - ALL LABOR AND MATERIALS NECESSARY FOR CONSTRUCTION AND MAINTENANCE OF TRAFFIC CONTROL DEVICES AND MARKINGS.
  - ALL FLAGPERSONS AND TRAFFIC CONTROL DEVICES SUCH AS, BUT NOT LIMITED TO, FLASHERS, SIGNS, BARRIER WALLS, BARRICADES, VERTICAL PANELS, PLASTIC DRUMS (STEEL DRUMS WILL NOT BE PERMITTED) AND CONES NECESSARY FOR THE CONTROL AND PROTECTION OF VEHICULAR AND PEDESTRIAN TRAFFIC AS SPECIFIED IN THESE NOTES, THE PLANS, THE MUTCD OR THE ENGINEER.
- ANY TEMPORARY TRAFFIC CONTROL ITEMS, DEVICES, MATERIALS AND INCIDENTALS SHALL REMAIN THE PROPERTY OF THE CONTRACTOR WHEN NO LONGER REQUIRED.
- THE CONTRACTOR SHALL MAINTAIN A TWO-LANE TRAVELLED WAY ON NEW HOLT ROAD, JAMES SANDERS BOULEVARD, THE KENTUCKY OAKS MALL ENTRANCE, AND THE WAL-MART ENTRANCE AT ALL TIMES. A MINIMUM OF TWO THROUGH LANES IN EACH DIRECTION SHALL BE MAINTAINED ON US 60 AT ALL TIMES. LEFT TURN STORAGE LANES SHALL BE PROVIDED AT ALL INTERSECTIONS WITH EXISTING LEFT TURN LANES. A MINIMUM LANE WIDTH OF 11 FEET SHALL BE MAINTAINED ON ALL ROADWAYS WITHIN THE PROJECT LIMITS AT ALL TIMES UNLESS OTHERWISE NOTED IN THE MAINTENANCE OF TRAFFIC PLAN AND/OR APPROVED BY THE ENGINEER.
- ALL EXISTING PEDESTRIAN WALKWAYS SHALL BE MAINTAINED OPEN AT ALL TIMES UNLESS OTHERWISE APPROVED BY THE ENGINEER. IN PLACES WHERE THE PROPOSED CONSTRUCTION INVOLVES THE REMOVAL OF EXISTING SIDEWALKS, WHERE POSSIBLE, THE EXISTING SIDEWALKS SHOULD BE KEPT OPEN TO PEDESTRIANS WHILE THE PROPOSED PEDESTRIAN FACILITIES ARE BUILT. IN LOCATIONS WHERE ADEQUATE USE OF EXISTING SIDEWALKS CANNOT BE MAINTAINED DUE TO CONSTRUCTION OPERATIONS, A TEMPORARY DETOUR NEEDS TO BE PROVIDED FOR PEDESTRIANS AS CLOSE AS POSSIBLE TO THE LOCATION OF ANY EXISTING SIDEWALKS BEFORE THESE ARE REMOVED. TEMPORARY PEDESTRIAN DETOURS SHALL COMPLY WITH ALL DESIGN STANDARDS AND SPECIFICATIONS REQUIRED FOR SIDEWALK CONSTRUCTION INCLUDING, AND NOT LIMITED TO, A MINIMUM WIDTH OF 5 FEET, A MAXIMUM 2% CROSS SLOPE, AND A MAXIMUM LONGITUDINAL GRADE OF 5%. SIDEWALK RAMP NEEDS TO BE PROVIDED AT THE INTERSECTION OF THE TEMPORARY PEDESTRIAN DETOURS WITH ALL ROADWAYS AND ENTRANCES. PROVIDE BARRICADES AND PROPER SIGNS AT THE BEGINNING AND END POINT OF SECTIONS OF EXISTING OR PROPOSED SIDEWALKS AND SHARED USE PATHS THAT ARE TO BE CLOSED TO PEDESTRIAN TRAFFIC. PAYMENT FOR TEMPORARY SIDEWALKS AND RECONSTRUCTION AT TEMPORARY CROSSINGS ARE CONSIDERED INCIDENTAL TO MAINTAINING AND CONTROLLING TRAFFIC.
- THE CONTRACTOR SHALL COMPLETELY COVER ANY SIGNS, EITHER EXISTING, PERMANENT OR TEMPORARY, WHICH DO NOT PROPERLY APPLY TO THE CURRENT TRAFFIC PHASING, AND SHALL MAINTAIN THE COVERING UNTIL THE SIGNS ARE APPLICABLE OR ARE REMOVED. THE CONTRACTOR MAY RELOCATE ANY EXISTING SIGN DISTURBED BY ACTIVE CONSTRUCTION WHICH REMAINS APPLICABLE WHILE CONSTRUCTION IS OCCURRING TO A LOCATION AS APPROVED BY THE ENGINEER.
- IN GENERAL, ALL TRAFFIC CONTROL DEVICES SHALL BE PLACED STARTING AND PROCEEDING IN THE DIRECTION OF THE FLOW OF TRAFFIC AND REMOVED STARTING AND PROCEEDING IN THE DIRECTION OPPOSITE THE FLOW OF TRAFFIC. TRAFFIC CONTROL DEVICES SHALL BE MOVED DURING CONSTRUCTION ACTIVITIES TO ACCOMMODATE THE GREATEST LANE WIDTH ALLOWABLE FOR ALL TRAVEL LANES. THE DEVICES WILL NEED TO BE MOVED THROUGHOUT ALL OPERATIONS DURING WORKING HOURS. ANY RELOCATION REQUIRED WILL NOT BE PAID FOR DIRECTLY, BUT WILL BE CONSIDERED INCIDENTAL TO MAINTAINING AND CONTROLLING TRAFFIC.
- THE ENGINEER AND CONTRACTOR, OR THEIR AUTHORIZED REPRESENTATIVES, SHALL REVIEW THE SIGNING BEFORE TRAFFIC IS ALLOWED TO USE ANY LANE CLOSURES, CROSSOVERS OR DETOURS. ALL SIGNING SHALL BE APPROVED BY THE ENGINEER BEFORE WORK CAN BE STARTED BY THE CONTRACTOR.
- IF THE CONTRACTOR DESIRES TO DEVIATE FROM THE TRAFFIC CONTROL SCHEME AND CONSTRUCTION SCHEDULE OUTLINED IN THESE PLANS AND THIS PROPOSAL, HE SHALL PREPARE AN ALTERNATE PLAN AND PRESENT IT IN WRITING TO THE ENGINEER. THIS ALTERNATE PLAN CAN BE USED ONLY AFTER REVIEW AND APPROVAL OF THE DIVISIONS OF TRAFFIC, DESIGN AND CONSTRUCTION, AND THE FEDERAL HIGHWAY ADMINISTRATION, WHERE APPLICABLE.
- IF TRAFFIC SHOULD BE STOPPED DUE TO CONSTRUCTION OPERATIONS AND AN EMERGENCY VEHICLE ON AN OFFICIAL EMERGENCY RUN ARRIVES AT THE SCENE, THE CONTRACTOR SHALL MAKE PROVISIONS FOR THE PASSAGE OF THAT VEHICLE AS QUICKLY AS POSSIBLE.
- THE SIGNALIZATION OF THE INTERSECTIONS WITHIN THE PROJECT LIMITS SHALL BE MAINTAINED AT ALL TIMES. BOTH PEDESTRIAN AND VEHICULAR SIGNALIZATION SCHEMES SHALL BE MAINTAINED. SIGNAL TIMING PLANS APPROVED OR MANDATED BY THE ENGINEER SHALL BE MAINTAINED AT THE INTERSECTIONS WITH A COMBINATION OF TEMPORARY AND/OR PERMANENT SIGNALIZATION EQUIPMENT. THE LOCATION AND TYPE OF TEMPORARY SIGNALIZATION EQUIPMENT MUST BE APPROVED BY THE ENGINEER, EXCEPT FOR THE HOURS OF 7 A.M. TO 9 P.M., THE SIGNALS MAY BE OPERATED MANUALLY FOR BRIEF PERIODS OF TIME IF APPROVED BY THE ENGINEER. THESE HOURS MAY BE EXTENDED BY THE ENGINEER IF SPECIAL EVENTS WARRANT. SIGNAL POLES AFFECTED BY ANY PHASE OF CONSTRUCTION WILL HAVE TEMPORARY POLES OR NEW POLES CONSTRUCTED BEFORE GRADING ACTIVITIES BEGIN OR ANY EXISTING SIGNAL POLE IS REMOVED. RELOCATION OF SIGNAL HEADS IF REQUIRED SHOULD BE PERFORMED DURING HOURS OF LOW TRAFFIC FLOW UTILIZING FLAGGERS AS NEEDED DURING THESE OPERATIONS. KYTC SHALL BE GIVEN 48 HOURS NOTICE BEFORE ANY SIGNALS ARE CHANGED OR RELOCATED.
- ALL EXISTING SIGNALIZATION EQUIPMENT REMAINS THE PROPERTY OF THE KYTC AND THE EQUIPMENT SHALL BE DELIVERED TO THE DISTRICT 1 TRAFFIC FACILITY, AS DIRECTED BY THE ENGINEER.

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DATE PLOTTED: November 30, 2018

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16. TEMPORARY SIGNALS WILL BE UTILIZED AT THE RAMP INTERSECTIONS DURING CONSTRUCTION. THE TEMPORARY SIGNALS SHALL MAINTAIN THE EXISTING TRAFFIC PATTERNS. TRAFFIC SHALL NOT BE SHIFTED TO THE ULTIMATE CONFIGURATION UNTIL PERMANENT SIGNALS ARE IN OPERATION AT THE PROPOSED CROSSOVERS. THE TEMPORARY SIGNALS SHALL BE COMPLETELY INDEPENDENT FROM THE EXISTING AND PROPOSED SIGNALS TO ALLOW FOR REMOVAL OF THE EXISTING SIGNALS AND CONSTRUCTION OF THE PROPOSED SIGNALS WITHOUT INTERRUPTING THE ACTIVE SIGNALIZATION AT ANY TIME.

17. PROPOSED MAST ARM POLES AND THEIR RESPECTIVE BASES WHICH HAVE BEEN CONSTRUCTED MAY BE SHIELDED WITH TEMPORARY CRASH CUSHIONS UNTIL SUCH TIME THAT TRAFFIC HAS BEEN SHIFTED TO THE ULTIMATE CONFIGURATIONS.

18. THE EXISTING LIGHTING ALONG THE INTERCHANGE RAMP SHALL REMAIN FUNCTIONAL AT ALL TIMES DURING CONSTRUCTION OF THE PROJECT WHILE THE PROPOSED PROJECT LIGHTING IS BUILT. TEMPORARY LIGHTING POLES SHALL BE PROVIDED AT NEARBY LOCATIONS WHERE PROPOSED CONSTRUCTION INTERFERES WITH THE EXISTING LIGHTING. PROPOSED LIGHTING CONSTRUCTION SHALL BE ACCOMPLISHED BY TEMPORARY SHOULDER CLOSURES AND TEMPORARY LANE CLOSURES. TEMPORARY LIGHTING SHALL BE INCIDENTAL TO THE COST OF PERMANENT LIGHTING.

19. EMERGENCY ACCESS TO FIRE HYDRANTS SHALL BE MAINTAINED AT ALL TIMES.

20. REASONABLE MEANS OF INGRESS AND EGRESS SHALL BE MAINTAINED TO ALL PROPERTIES WITHIN THE PROJECT LIMITS AT ALL TIMES.

21. ON DIFFERENT NIGHTS / WEEKENDS OF PHASE II CONSTRUCTION, RAMP 'A' AND 'C' MAY BE CLOSED FOR A PERIOD OF ONE EXTENDED WEEKEND (OR AT THE DISCRETION OF THE ENGINEER) BEGINNING AT 7:00 PM FRIDAY UNTIL THE FOLLOWING MONDAY AT 6:00 AM. DURING PHASE III OF CONSTRUCTION, RAMP 'D' MAY BE CLOSED INDEPENDENTLY FOR A PERIOD OF ONE EXTENDED WEEKEND (OR AT THE DISCRETION OF THE ENGINEER) BEGINNING AT 7:00 PM FRIDAY UNTIL THE FOLLOWING MONDAY AT 6:00 AM. REMOVE THE EXISTING PAVEMENT AND CONSTRUCT THIS RAMP TO THE TOP BASE COURSE, AS DEPICTED IN THE MAINTENANCE OF TRAFFIC PLAN. LIQUIDATED DAMAGES OF \$5000.00/HOUR WILL BE ASSESSED UNTIL THESE ITEMS OF WORK ARE COMPLETE AND THE RAMP REOPENED.

22. THE FINAL STRIPING CONFIGURATION WILL BE PLACED TWICE DURING CONSTRUCTION. ONCE WHEN COMPLETING THE PROPOSED SCRATCH AND BASE COURSES AND REPEATED UPON COMPLETION OF THE FINAL SURFACE COURSE. THE PROPOSED CURB PAINTING WILL ONLY BE PLACED ONCE DURING PHASE V OF CONSTRUCTION.

23. THE ENGINEER AND CONTRACTOR, OR THEIR AUTHORIZED REPRESENTATIVES, SHALL COORDINATE CONSTRUCTION SEQUENCING WITH ALL LOCAL LAW ENFORCEMENT AND EMERGENCY AGENCIES.

24. AS APPROVED BY THE ENGINEER, BLACK PAINT MAY BE UTILIZED TO ELIMINATE EXISTING STRIPING IN LIEU OF WATERBLASTING. BLACK PAINT WILL BE PAID FOR AS LINEAR FEET OF TEMPORARY PAVEMENT STRIPING - 4 INCH AND 6 INCH. BLACK PAINT SHALL NOT BE PERMITTED ON FINAL ASPHALT SURFACES.

25. UPON COMPLETION OF THE INTERCHANGE CLOSURE OF PHASE IV CONSTRUCTION, TRAFFIC SHALL BE SHIFTED TO THE ULTIMATE CONFIGURATION (ALL LANES OPEN TO TRAFFIC THROUGH THE DCD INTERCHANGE FROM JAMES SANDERS BOULEVARD TO COLEMAN ROAD INCLUDING RAMP). LIQUIDATED DAMAGES OF \$5000.00/HOUR WILL BE ASSESSED UNTIL THIS HAS OCCURRED.

26. THE FIXED COMPLETION DATE FOR THE PROJECT SHALL BE NOVEMBER 19, 2019.

## PAVEMENT DROP-OFF NOTES

A PAVEMENT EDGE THAT TRAFFIC IS EXPECTED TO CROSS IN A LANE CHANGE SITUATION SHALL NOT HAVE AN ELEVATION DIFFERENCE GREATER THAN 1 1/2 INCHES. THIS MAY BE INCREASED TO 2 INCHES FOR LOW SPEED SITUATIONS. WARNING SIGNS SHALL BE PLACED IN ADVANCE AND THROUGHOUT THE DROP-OFF AREA WHEN DROP-OFFS ARE GREATER THAN 1/2 INCH. MODIFICATIONS WILL BE AS DIRECTED BY THE ENGINEER.

A PAVEMENT EDGE THAT TRAFFIC IS NOT EXPECTED TO CROSS, EXCEPT ACCIDENTALLY, SHOULD BE TREATED AS FOLLOWS:

LESS THAN TWO INCHES NO PROTECTION REQUIRED. WARNING SIGNS PLACED IN ADVANCE AND THROUGHOUT THE DROP-OFF AREA.

TWO TO FOUR INCHES PLASTIC DRUMS, VERTICAL PANELS OR BARRICADES EVERY 100 FEET ON TANGENT SECTIONS FOR SPEEDS OF 50 MPH OR GREATER. CONES MAY BE USED IN PLACE OF PLASTIC DRUMS, PANELS AND BARRICADES DURING DAYLIGHT HOURS. FOR TANGENT SECTIONS WITH SPEED LESS THAN 50 MPH AND FOR CURVES, DEVICES SHOULD BE PLACED EVERY 25 FEET. SPACING OF DEVICES ON TAPERED SECTIONS SHOULD BE IN ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, CURRENT EDITION.

GREATER THAN FOUR INCHES POSITIVE SEPARATION OR WEDGE WITH 3:1 OR FLATTER SLOPE NEEDED. IF THERE IS FIVE FEET OR MORE DISTANCE BETWEEN THE EDGE OF THE PAVEMENT AND THE DROP-OFF, THEN DRUMS, PANELS OR BARRICADES MAY BE USED. IF THE DROP-OFF IS GREATER THAN 12 INCHES, POSITIVE SEPARATION IS STRONGLY ENCOURAGED. IF CONCRETE BARRIERS ARE USED, SPECIAL REFLECTIVE DEVICES OR STEADY BURN LIGHTS SHOULD BE USED FOR OVERNIGHT INSTALLATIONS.

FOR TEMPORARY CONDITIONS, DROP-OFFS GREATER THAN FOUR INCHES MAY BE PROTECTED WITH PLASTIC DRUMS, VERTICAL PANELS OR BARRICADES FOR SHORT DISTANCES DURING DAYLIGHT HOURS WHILE WORK IS BEING DONE IN THE DROP-OFF AREA.

LESSER TREATMENTS THAN THOSE DESCRIBED ABOVE MAY BE CONSIDERED FOR LOW-VOLUME LOCAL STREETS.

PAYMENT WILL BE ALLOWED FOR DGA MATERIAL USED FOR WEDGING.

## TRAFFIC COORDINATOR

THE CONTRACTOR SHALL DESIGNATE AN EMPLOYEE OR EMPLOYEES TO BE TRAFFIC COORDINATOR(S). THE TRAFFIC COORDINATOR SHALL INSPECT THE PROJECT MAINTENANCE OF TRAFFIC AT LEAST ONCE A DAY, MONDAY THROUGH FRIDAY, AND TWICE A DAY (MORNING AND EVENING) SATURDAY AND SUNDAY, FOR THE LIFE OF THE PROJECT. ADDITIONALLY, THE TRAFFIC COORDINATOR SHALL REPORT ALL INCIDENTS THROUGHOUT THE WORK ZONE TO THE ENGINEER ON THE PROJECT. A TRAFFIC COORDINATOR SHALL BE ON THE PROJECT AT ALL TIMES WHEN LANE CLOSURES ARE IN USE TO INSPECT THE TRAFFIC CONTROL, MAINTAIN THE SIGNING AND DEVICES AND RELOCATE VARIABLE MESSAGE BOARDS AS NEEDED OR AS DIRECTED BY THE ENGINEER. A TRAFFIC COORDINATOR SHALL BE ON CALL 24 HOURS A DAY, 7 DAYS A WEEK FOR THE PROJECT DURATION. THE TRAFFIC COORDINATOR SHALL BE RESPONSIBLE FOR TRAFFIC CONTROL MAINTENANCE AND SHALL MAKE AT LEAST ONE PASS-THROUGH INSPECTION ON THE PROJECT PER HOUR AT ALL TIMES LANES ARE CLOSED. THE CONTRACTOR SHALL FURNISH THE NAME AND TELEPHONE NUMBER WHERE THE TRAFFIC COORDINATOR CAN BE CONTACTED AT ANY TIME. THE TRAFFIC COORDINATOR SHALL HAVE ACCESS ON THE PROJECT TO A RADIO OR TELEPHONE TO BE USED IN CASE OF EMERGENCIES OR ACCIDENTS.

## TEMPORARY PAVEMENT

TEMPORARY PAVEMENT SHALL CONSIST OF:

- 1.25" CL 2 ASPH SURF 0.380 PG 64-22
- 3.50" CL 2 ASPH BASE 1.00 PG 64-22
- 3.50" CL 2 ASPH BASE 1.00 PG 64-22
- 4.00" CRUSHED STONE BASE

US 60  
MAINTENANCE OF TRAFFIC PLAN  
GENERAL NOTES AND PROJECT NOTES



# MAINTENANCE OF TRAFFIC NOTES

## GENERAL TRAFFIC CONTROL NOTES

- TRAFFIC SHALL BE MAINTAINED IN ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD), THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND THE STANDARD DRAWINGS, CURRENT EDITIONS.
- ALL TEMPORARY TRAFFIC CONTROL DEVICES SHALL COMPLY WITH THE NATIONAL COOPERATIVE HIGHWAY RESEARCH PROGRAM (NCHRP) 350 REPORT.
- LANE CLOSURES SHALL NOT BE ALLOWED DURING THE HOURS OF 6:00 AM TO 7:00 PM MONDAY THROUGH FRIDAY. FURTHER, THEY SHALL NOT BE ALLOWED DURING ALL HOLIDAYS IDENTIFIED IN SECTION 101 OF THE STANDARD SPECIFICATIONS (INDEPENDENCE DAY, LABOR DAY & VETERAN'S DAY) OR SPECIAL EVENTS INCLUDING THE AOS PADUCAH QUILT SHOW.  
LISTED BELOW ARE DATES AND TIME FOR HOLIDAYS WHEN LANE CLOSURES WILL NOT BE ALLOWED.  
EASTER 6:00 AM THUR TO 9:00 PM MON  
MEMORIAL DAY 6:00 AM THUR TO 9:00 PM TUES  
JULY 4TH 6:00 AM JULY 1ST TO 9:00 PM JULY 7TH  
LABOR DAY 6:00 AM THUR TO 9:00 PM TUES
- THE PROPOSED DRAINAGE INLETS ON THIS PROJECT SHALL HAVE WEEP HOLES, BLEEDERS OR ANOTHER ACCEPTABLE FORM OF TEMPORARY DRAINAGE INSTALLED TO ALLOW FOR POSITIVE DRAINAGE DURING CONSTRUCTION PHASING.
- EXCEPT FOR THE ROADWAY AND TRAFFIC CONTROL BID ITEMS LISTED, ALL ITEMS OF WORK NECESSARY TO MAINTAIN AND CONTROL TRAFFIC WILL BE PAID FOR AT THE LUMP SUM PRICE TO MAINTAIN AND CONTROL TRAFFIC AS SET FORTH IN THE CURRENT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION UNLESS OTHERWISE PROVIDED FOR IN THESE NOTES BUT IS NOT LIMITED TO THE FOLLOWING ITEMS AND OPERATIONS:
  - ALL GRADING AND NECESSARY DRAINAGE (UNLESS A BID ITEM FOR DETOUR CONSTRUCTION IS INCLUDED) FOR THE TEMPORARY ROADWAY AND REMOVAL THEREOF, WHEN IT IS NO LONGER REQUIRED, IF A BID ITEM FOR DETOUR CONSTRUCTION IS INCLUDED, GRADING AND DRAINAGE WILL BE PAID FOR IN THE BID ITEM "DETOUR CONSTRUCTION".
  - ALL LABOR AND MATERIALS NECESSARY FOR CONSTRUCTION AND MAINTENANCE OF TRAFFIC CONTROL DEVICES AND MARKINGS.
  - ALL FLAGPERSONS AND TRAFFIC CONTROL DEVICES SUCH AS, BUT NOT LIMITED TO, FLASHERS, SIGNS, BARRIER WALLS, BARRICADES, VERTICAL PANELS, PLASTIC DRUMS (STEEL DRUMS WILL NOT BE PERMITTED) AND CONES NECESSARY FOR THE CONTROL AND PROTECTION OF VEHICULAR AND PEDESTRIAN TRAFFIC AS SPECIFIED IN THESE NOTES, THE PLANS, THE MUTCD OR THE ENGINEER.
- ANY TEMPORARY TRAFFIC CONTROL ITEMS, DEVICES, MATERIALS AND INCIDENTALS SHALL REMAIN THE PROPERTY OF THE CONTRACTOR WHEN NO LONGER REQUIRED.
- THE CONTRACTOR SHALL MAINTAIN A TWO-LANE TRAVELLED WAY ON NEW HOLT ROAD, JAMES SANDERS BOULEVARD, THE KENTUCKY OAKS MALL ENTRANCE, AND THE WAL-MART ENTRANCE AT ALL TIMES. A MINIMUM OF TWO THROUGH LANES IN EACH DIRECTION SHALL BE MAINTAINED ON US 60 AT ALL TIMES. LEFT TURN STORAGE LANES SHALL BE PROVIDED AT ALL INTERSECTIONS WITH EXISTING LEFT TURN LANES. A MINIMUM LANE WIDTH OF 11 FEET SHALL BE MAINTAINED ON ALL ROADWAYS WITHIN THE PROJECT LIMITS AT ALL TIMES UNLESS OTHERWISE NOTED IN THE MAINTENANCE OF TRAFFIC PLAN AND/OR APPROVED BY THE ENGINEER.
- ALL EXISTING PEDESTRIAN WALKWAYS SHALL BE MAINTAINED OPEN AT ALL TIMES UNLESS OTHERWISE APPROVED BY THE ENGINEER. IN PLACES WHERE THE PROPOSED CONSTRUCTION INVOLVES THE REMOVAL OF EXISTING SIDEWALKS, WHERE POSSIBLE, THE EXISTING SIDEWALKS SHOULD BE KEPT OPEN TO PEDESTRIANS WHILE THE PROPOSED PEDESTRIAN FACILITIES ARE BUILT. IN LOCATIONS WHERE ADEQUATE USE OF EXISTING SIDEWALKS CANNOT BE MAINTAINED DUE TO CONSTRUCTION OPERATIONS, A TEMPORARY DETOUR NEEDS TO BE PROVIDED FOR PEDESTRIANS AS CLOSE AS POSSIBLE TO THE LOCATION OF ANY EXISTING SIDEWALKS BEFORE THESE ARE REMOVED. TEMPORARY PEDESTRIAN DETOURS SHALL COMPLY WITH ALL DESIGN STANDARDS AND SPECIFICATIONS REQUIRED FOR SIDEWALK CONSTRUCTION INCLUDING, AND NOT LIMITED TO, A MINIMUM WIDTH OF 5 FEET, A MAXIMUM 2% CROSS SLOPE, AND A MAXIMUM LONGITUDINAL GRADE OF 5%. SIDEWALK RAMP NEEDS TO BE PROVIDED AT THE INTERSECTION OF THE TEMPORARY PEDESTRIAN DETOURS WITH ALL ROADWAYS AND ENTRANCES. PROVIDE BARRICADES AND PROPER SIGNS AT THE BEGINNING AND END POINT OF SECTIONS OF EXISTING OR PROPOSED SIDEWALKS AND SHARED USE PATHS THAT ARE TO BE CLOSED TO PEDESTRIAN TRAFFIC. PAYMENT FOR TEMPORARY SIDEWALKS AND RECONSTRUCTION AT TEMPORARY CROSSINGS ARE CONSIDERED INCIDENTAL TO MAINTAINING AND CONTROLLING TRAFFIC.
- THE CONTRACTOR SHALL COMPLETELY COVER ANY SIGNS, EITHER EXISTING, PERMANENT OR TEMPORARY, WHICH DO NOT PROPERLY APPLY TO THE CURRENT TRAFFIC PHASING, AND SHALL MAINTAIN THE COVERING UNTIL THE SIGNS ARE APPLICABLE OR ARE REMOVED. THE CONTRACTOR MAY RELOCATE ANY EXISTING SIGN DISTURBED BY ACTIVE CONSTRUCTION WHICH REMAINS APPLICABLE WHILE CONSTRUCTION IS OCCURRING TO A LOCATION AS APPROVED BY THE ENGINEER.
- IN GENERAL, ALL TRAFFIC CONTROL DEVICES SHALL BE PLACED STARTING AND PROCEEDING IN THE DIRECTION OF THE FLOW OF TRAFFIC AND REMOVED STARTING AND PROCEEDING IN THE DIRECTION OPPOSITE THE FLOW OF TRAFFIC. TRAFFIC CONTROL DEVICES SHALL BE MOVED DURING CONSTRUCTION ACTIVITIES TO ACCOMMODATE THE GREATEST LANE WIDTH ALLOWABLE FOR ALL TRAVEL LANES. THE DEVICES WILL NEED TO BE COVERED THROUGHOUT ALL OPERATIONS DURING WORKING HOURS. ANY RELOCATION REQUIRED WILL NOT BE PAID FOR DIRECTLY, BUT WILL BE CONSIDERED INCIDENTAL TO MAINTAINING AND CONTROLLING TRAFFIC.
- THE ENGINEER AND CONTRACTOR, OR THEIR AUTHORIZED REPRESENTATIVES, SHALL REVIEW THE SIGNING BEFORE TRAFFIC IS ALLOWED TO USE ANY LANE CLOSURES, CROSsoVERS OR DETOURS. ALL SIGNING SHALL BE APPROVED BY THE ENGINEER BEFORE WORK CAN BE STARTED BY THE CONTRACTOR.
- IF THE CONTRACTOR DESIRES TO DEVIATE FROM THE TRAFFIC CONTROL SCHEME AND CONSTRUCTION SCHEDULE OUTLINED IN THESE PLANS AND THIS PROPOSAL, HE SHALL PREPARE AN ALTERNATE PLAN AND PRESENT IT IN WRITING TO THE ENGINEER. THIS ALTERNATE PLAN CAN BE USED ONLY AFTER REVIEW AND APPROVAL OF THE DIVISIONS OF TRAFFIC, DESIGN AND CONSTRUCTION, AND THE FEDERAL HIGHWAY ADMINISTRATION, WHERE APPLICABLE.
- IF TRAFFIC SHOULD BE STOPPED DUE TO CONSTRUCTION OPERATIONS AND AN EMERGENCY VEHICLE ON AN OFFICIAL EMERGENCY RUN ARRIVES AT THE SCENE, THE CONTRACTOR SHALL MAKE PROVISIONS FOR THE PASSAGE OF THAT VEHICLE AS QUICKLY AS POSSIBLE.
- THE SIGNALIZATION OF THE INTERSECTIONS WITHIN THE PROJECT LIMITS SHALL BE MAINTAINED AT ALL TIMES. BOTH PEDESTRIAN AND VEHICULAR SIGNALIZATION SCHEMES SHALL BE MAINTAINED. SIGNAL TIMING PLANS APPROVED OR MANDATED BY THE ENGINEER SHALL BE MAINTAINED AT THE INTERSECTIONS WITH A COMBINATION OF TEMPORARY AND/OR PERMANENT SIGNALIZATION EQUIPMENT. THE LOCATION AND TYPE OF TEMPORARY SIGNALIZATION EQUIPMENT MUST BE APPROVED BY THE ENGINEER, EXCEPT FOR THE HOURS OF 7 A.M. TO 9 P.M., THE SIGNALS MAY BE OPERATED MANUALLY FOR BRIEF PERIODS OF TIME IF APPROVED BY THE ENGINEER. THESE HOURS MAY BE EXTENDED BY THE ENGINEER IF SPECIAL EVENTS WARRANT. SIGNAL POLES AFFECTED BY ANY PHASE OF CONSTRUCTION WILL HAVE TEMPORARY POLES OR NEW POLES CONSTRUCTED BEFORE GRADING ACTIVITIES BEGIN OR ANY EXISTING SIGNAL POLE IS REMOVED. RELOCATION OF SIGNAL HEADS IF REQUIRED SHOULD BE PERFORMED DURING HOURS OF LOW TRAFFIC FLOW UTILIZING FLAGGERS AS NEEDED DURING THESE OPERATIONS. KYTC SHALL BE GIVEN 48 HOURS NOTICE BEFORE ANY SIGNALS ARE CHANGED OR RELOCATED.
- ALL EXISTING SIGNALIZATION EQUIPMENT REMAINS THE PROPERTY OF THE KYTC AND THE EQUIPMENT SHALL BE DELIVERED TO THE DISTRICT 1 TRAFFIC FACILITY, AS DIRECTED BY THE ENGINEER.

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USER: kdeep

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- TEMPORARY SIGNALS WILL BE UTILIZED AT THE RAMP INTERSECTIONS DURING CONSTRUCTION. THE TEMPORARY SIGNALS SHALL MAINTAIN THE EXISTING TRAFFIC PATTERNS. TRAFFIC SHALL NOT BE SHIFTED TO THE ULTIMATE CONFIGURATION UNTIL PERMANENT SIGNALS ARE IN OPERATION AT THE PROPOSED CROSsoVERS. THE TEMPORARY SIGNALS SHALL BE COMPLETELY INDEPENDENT FROM THE EXISTING AND PROPOSED SIGNALS TO ALLOW FOR REMOVAL OF THE EXISTING SIGNALS AND CONSTRUCTION OF THE PROPOSED SIGNALS WITHOUT INTERRUPTING THE ACTIVE SIGNALIZATION AT ANY TIME.
- PROPOSED MAST ARM POLES AND THEIR RESPECTIVE BASES WHICH HAVE BEEN CONSTRUCTED MAY BE SHIELDED WITH TEMPORARY CRASH CUSHIONS UNTIL SUCH TIME THAT TRAFFIC HAS BEEN SHIFTED TO THE ULTIMATE CONFIGURATIONS.
- THE EXISTING LIGHTING ALONG THE INTERCHANGE RAMPs SHALL REMAIN FUNCTIONAL AT ALL TIMES DURING CONSTRUCTION OF THE PROJECT WHILE THE PROPOSED PROJECT LIGHTING IS BUILT. TEMPORARY LIGHTING POLES SHALL BE PROVIDED AT NEARBY LOCATIONS WHERE PROPOSED CONSTRUCTION INTERFERES WITH THE EXISTING LIGHTING. PROPOSED LIGHTING CONSTRUCTION SHALL BE ACCOMPLISHED BY TEMPORARY SHOULDER CLOSURES AND TEMPORARY LANE CLOSURES. TEMPORARY LIGHTING SHALL BE INCIDENTAL TO THE COST OF PERMANENT LIGHTING.
- EMERGENCY ACCESS TO FIRE HYDRANTS SHALL BE MAINTAINED AT ALL TIMES.
- REASONABLE MEANS OF INGRESS AND EGRESS SHALL BE MAINTAINED TO ALL PROPERTIES WITHIN THE PROJECT LIMITS AT ALL TIMES.
- ON DIFFERENT NIGHTS WEEKENDS OF PHASE I CONSTRUCTION, RAMPs 'A' AND 'C' MAY BE CLOSED FOR A PERIOD OF ONE EXTENDED WEEKEND (OR AT THE DISCRETION OF THE ENGINEER) BEGINNING AT 7:00 PM FRIDAY UNTIL THE FOLLOWING MONDAY AT 6:00 AM. DURING PHASE III OF CONSTRUCTION, RAMP 'D' MAY BE CLOSED INDEPENDENTLY FOR A PERIOD OF ONE EXTENDED WEEKEND (OR AT THE DISCRETION OF THE ENGINEER) BEGINNING AT 7:00 PM FRIDAY UNTIL THE FOLLOWING MONDAY AT 6:00 AM. REMOVE THE EXISTING PAVEMENT AND CONSTRUCT THIS RAMP TO THE TOP BASE COURSE, AS DEPICTED IN THE MAINTENANCE OF TRAFFIC PLAN. LIQUIDATED DAMAGES OF \$5000.00/HOUR WILL BE ASSESSED UNTIL THESE ITEMS OF WORK ARE COMPLETE AND THE RAMPs REOPENED.
- THE FINAL STRIPING CONFIGURATION WILL BE PLACED TWICE DURING CONSTRUCTION. ONCE WHEN COMPLETING THE PROPOSED SCRATCH AND BASE COURSES AND REPEATED UPON COMPLETION OF THE FINAL SURFACE COURSE. THE PROPOSED CURB PAINTING WILL ONLY BE PLACED ONCE DURING PHASE V OF CONSTRUCTION.
- THE ENGINEER AND CONTRACTOR, OR THEIR AUTHORIZED REPRESENTATIVES, SHALL COORDINATE CONSTRUCTION SEQUENCING WITH ALL LOCAL LAW ENFORCEMENT AND EMERGENCY AGENCIES.
- AS APPROVED BY THE ENGINEER, BLACK PAINT MAY BE UTILIZED TO ELIMINATE EXISTING STRIPING IN LIEU OF WATERBLASTING. BLACK PAINT WILL BE PAID FOR AS LINEAR FEET OF TEMPORARY PAVEMENT STRIPING - 4 INCH AND 6 INCH. BLACK PAINT SHALL NOT BE PERMITTED ON FINAL ASPHALT SURFACES.
- UPON COMPLETION OF THE INTERCHANGE CLOSURE OF PHASE IV CONSTRUCTION, TRAFFIC SHALL BE SHIFTED TO THE ULTIMATE CONFIGURATION ~~WALK LINES OPEN TO TRAFFIC THROUGH THE DOOR INTERCHANGE FROM JAMES SANDERS BOULEVARD TO COLEMAN ROAD INCLUDING RAMPs.~~ LIQUIDATED DAMAGES OF \$5000.00/HOUR WILL BE ASSESSED UNTIL THIS HAS OCCURRED.
- THE FIXED COMPLETION DATE FOR THE PROJECT SHALL BE NOVEMBER 19, 2019.

## PAVEMENT DROP-OFF NOTES

- A PAVEMENT EDGE THAT TRAFFIC IS EXPECTED TO CROSS IN A LANE CHANGE SITUATION SHALL NOT HAVE AN ELEVATION DIFFERENCE GREATER THAN 1 1/2 INCHES. THIS MAY BE INCREASED TO 2 INCHES FOR LOW SPEED SITUATIONS. WARNING SIGNS SHALL BE PLACED IN ADVANCE AND THROUGHOUT THE DROP-OFF AREA WHEN DROP-OFFS ARE GREATER THAN 1/2 INCH. MODIFICATIONS WILL BE AS DIRECTED BY THE ENGINEER.
- A PAVEMENT EDGE THAT TRAFFIC IS NOT EXPECTED TO CROSS, EXCEPT ACCIDENTALLY, SHOULD BE TREATED AS FOLLOWS:
  - LESS THAN TWO INCHES NO PROTECTION REQUIRED. WARNING SIGNS PLACED IN ADVANCE AND THROUGHOUT THE DROP-OFF AREA.
  - TWO TO FOUR INCHES PLASTIC DRUMS, VERTICAL PANELS OR BARRICADES EVERY 100 FEET ON TANGENT SECTIONS FOR SPEEDS OF 50 MPH OR GREATER. CONES MAY BE USED IN PLACE OF PLASTIC DRUMS, PANELS AND BARRICADES DURING DAYLIGHT HOURS. FOR TANGENT SECTIONS WITH SPEED LESS THAN 50 MPH AND FOR CURVES, DEVICES SHOULD BE PLACED EVERY 25 FEET. SPACING OF DEVICES ON TAPERED SECTIONS SHOULD BE IN ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, CURRENT EDITION.
  - GREATER THAN FOUR INCHES POSITIVE SEPARATION OR WEDGE WITH 3:1 OR FLATTER SLOPE NEEDED. IF THERE IS FIVE FEET OR MORE DISTANCE BETWEEN THE EDGE OF THE PAVEMENT AND THE DROP-OFF, THEN DRUMS, PANELS OR BARRICADES MAY BE USED. IF THE DROP-OFF IS GREATER THAN 12 INCHES, POSITIVE SEPARATION IS STRONGLY ENCOURAGED. IF CONCRETE BARRIERS ARE USED, SPECIAL REFLECTIVE DEVICES OR STEADY BURN LIGHTS SHOULD BE USED FOR OVERNIGHT INSTALLATIONS.
- FOR TEMPORARY CONDITIONS, DROP-OFFS GREATER THAN FOUR INCHES MAY BE PROTECTED WITH PLASTIC DRUMS, VERTICAL PANELS OR BARRICADES FOR SHORT DISTANCES DURING DAYLIGHT HOURS WHILE WORK IS BEING DONE IN THE DROP-OFF AREA.

LESSER TREATMENTS THAN THOSE DESCRIBED ABOVE MAY BE CONSIDERED FOR LOW-VOLUME LOCAL STREETS.

PAYMENT WILL BE ALLOWED FOR DGA MATERIAL USED FOR WEDGING.

## TRAFFIC COORDINATOR

THE CONTRACTOR SHALL DESIGNATE AN EMPLOYEE OR EMPLOYEES TO BE TRAFFIC COORDINATOR(S). THE TRAFFIC COORDINATOR SHALL INSPECT THE PROJECT MAINTENANCE OF TRAFFIC AT LEAST ONCE A DAY, MONDAY THROUGH FRIDAY, AND TWICE A DAY (MORNING AND EVENING) SATURDAY AND SUNDAY, FOR THE LIFE OF THE PROJECT. ADDITIONALLY, THE TRAFFIC COORDINATOR SHALL REPORT ALL INCIDENTS THROUGHOUT THE WORK ZONE TO THE ENGINEER ON THE PROJECT. A TRAFFIC COORDINATOR SHALL BE ON THE PROJECT AT ALL TIMES WHEN LANE CLOSURES ARE IN USE TO INSPECT THE TRAFFIC CONTROL, MAINTAIN THE SIGNING AND DEVICES AND RELOCATE VARIABLE MESSAGE BOARDS AS NEEDED OR AS DIRECTED BY THE ENGINEER. A TRAFFIC COORDINATOR SHALL BE ON CALL 24 HOURS A DAY, 7 DAYS A WEEK FOR THE PROJECT DURATION. THE TRAFFIC COORDINATOR SHALL BE RESPONSIBLE FOR TRAFFIC CONTROL MAINTENANCE AND SHALL MAKE AT LEAST ONE PASS-THROUGH INSPECTION ON THE PROJECT PER HOUR AT ALL TIMES LANES ARE CLOSED. THE CONTRACTOR SHALL FURNISH THE NAME AND TELEPHONE NUMBER WHERE THE TRAFFIC COORDINATOR CAN BE CONTACTED AT ANY TIME. THE TRAFFIC COORDINATOR SHALL HAVE ACCESS ON THE PROJECT TO A RADIO OR TELEPHONE TO BE USED IN CASE OF EMERGENCIES OR ACCIDENTS.

## TEMPORARY PAVEMENT

TEMPORARY PAVEMENT SHALL CONSIST OF:

- 1.25" CL 2 ASPH SURF 0.380 PG 64-22
- 3.50" CL 2 ASPH BASE 1.00 PG 64-22
- 3.50" CL 2 ASPH BASE 1.00 PG 64-22
- 4.00" CRUSHED STONE BASE

# MAINTENANCE OF TRAFFIC NOTES

COUNTY OF	ITEM NO.	SHEET NO.
MCCRACKEN	1-154.00	R71

## PHASE III CONSTRUCTION

SECTION 1 - FROM THE PROJECT BEGINNING TO THE JAMES SANDERS BOULEVARD INTERSECTION SHIFT US 60 TRAFFIC LANES TO PREVIOUSLY CONSTRUCTED OUTSIDE LANES AND CONSTRUCT RAISED BARRIER MEDIAN ON US 60 AND PAINTED ISLANDS IN THE ENTRANCES.

LEAVE THE TOP 1.5 INCH ASPHALT SURFACE TO BE COMPLETED IN THE LAST PHASE OF THE PROJECT.

NON-ESSENTIAL WORK ITEMS ON THIS SECTION CAN BE COMPLETED DURING LATER PHASES. HOWEVER, PHASE III WORK SHALL BE SUBSTANTIALLY COMPLETED, WITH THE ULTIMATE NUMBER OF LANES OPEN TO TRAFFIC, TEMPORARY STRIPING DEPICTING THE FINAL STRIPING CONFIGURATION AS SHOWN IN THE ULTIMATE STRIPING PLANS. ULTIMATE SIGNING IN PLACE, FINAL PROJECT LIGHTING AND THE FINAL PROPOSED SIGNALS IN OPERATION ALONG THIS SECTION, BEFORE THE CROSSOVER INTERSECTIONS CAN BE OPENED TO TRAFFIC IN ITS ULTIMATE CONFIGURATION AT THE BEGINNING OF PHASE IV.

SECTION 2 - FROM THE JAMES SANDERS BOULEVARD INTERSECTION TO PROJECT END FIRST, OPEN NEWLY-CONSTRUCTED RAMPS A AND C TO TRAFFIC.

PHASE III WORK ALONG THIS SECTION SHALL CONSIST OF THE COMPLETION OF ANY REMAINING DRAINAGE ITEMS, RAISED MEDIANS, REMAINING PORTIONS OF THE PROPOSED GRADING, CONSTRUCTION OF ANY REMAINING PORTIONS OF MILLING AND LEVELING AND WEDGING FOR CROSS SLOPE CORRECTION, AND TEMPORARY STRIPING. THIS WORK SHALL BE PERFORMED USING MOVEABLE LANE CLOSURE OPERATIONS WITH FLAGGERS AND SHALL BE PERFORMED DURING PERIODS OF LOW TRAFFIC FLOW OR AT TIMES APPROVED BY THE ENGINEER. TEMPORARY STRIPING SHALL BE PLACED AS NEEDED DURING THESE OPERATIONS AND AT ANY TIME WORK CEASES.

WHEN THE TEMPORARY STRIPING IS IN PLACE, SHIFT TRAFFIC TOWARDS THE OUTER LANES ON BOTH EASTBOUND AND WESTBOUND DIRECTIONS AS SHOWN IN THE PLANS, AWAY FROM THE PROPOSED CENTER RAISED MEDIANS. AT THIS POINT, TRAFFIC IS STILL BEING MAINTAINED UNDER THE NORMAL DIAMOND INTERCHANGE CONFIGURATION, SIMILARLY TO THE EXISTING CONFIGURATION.

MAINTAIN THE EXISTING OR TEMPORARY SIGNALS IN PLACE TO CONTROL INTERCHANGE TRAFFIC AT THE END OF THE RAMPS DURING PHASE III WORK. INSTALL ALL PROPOSED SIGNAL MAST ARM ASSEMBLIES FOR THE CROSSOVER INTERSECTIONS (EXCEPT FOR PROPOSED SIGNALS 56 FEET RT. OF STA. 204+02 AND 49 FEET RT. OF STA. 204+57.) INTENDED FOR THE ULTIMATE CONFIGURATION. TEMPORARY CRASH CUSHIONS MAY BE UTILIZED TO PROTECT THE PROPOSED CROSSOVER SIGNAL MAST ARM POLES AND BASES IN THE FUTURE EXIT RAMP ISLANDS, UNTIL TRAFFIC IS SHIFTED TO THE ULTIMATE CONFIGURATION AND THE RAMP ISLAND CURBS HAVE BEEN CONSTRUCTED.

DURING PHASE III, RAMP 'D' MAY BE CLOSED INDEPENDENTLY FOR A PERIOD OF ONE EXTENDED WEEKEND OR AT THE DISCRETION OF THE ENGINEER BEGINNING AT 7:00 PM FRIDAY UNTIL THE FOLLOWING MONDAY AT 6:00 AM. REMOVE THE EXISTING PAVEMENT AND CONSTRUCT THE REMAINING PORTIONS OF RAMP D AND D-1, AS DEPICTED IN THE MAINTENANCE OF TRAFFIC PLAN. LIQUIDATED DAMAGES OF \$5000.00/HOUR WILL BE ASSESSED UNTIL THESE ITEMS OF WORK ARE COMPLETE AND THE RAMPS REOPENED.

## PHASE IV CONSTRUCTION

PHASE IV WORK INVOLVES FINAL CONSTRUCTION ITEMS WITHIN THE LIMITS OF THE CROSSOVER INTERSECTIONS. DURING THIS PHASE, THE CONTRACTOR WILL BE ALLOCATED ONE WEEKEND BEGINNING AT 7:00 PM FRIDAY AND ENDING AT 6:00 AM MONDAY TO CLOSE THE SECTION OF US 60 MAINLINE ROADWAY FROM THE JAMES SANDERS BOULEVARD INTERSECTION TO THE COLEMAN ROAD INTERSECTION. DURING THIS PERIOD THE CONTRACTOR SHALL COMPLETE ANY ITEMS THAT WERE LEFT INCOMPLETE DURING PREVIOUS PHASES OF THE PROJECT TO FACILITATE ULTIMATE TRAFFIC CONTROL OPERATIONS. THE FINAL STAGE OF THIS PHASE INCLUDES THE PLACEMENT OF A 0.5" SCRATCH COURSE. THIS COURSE SHALL BE PLACED OVER MAINLINE US 60 AND THE ENTRANCE AND EXIT RAMPS TO I-24. THE SCRATCH COURSE WILL BE STRIPED WITH THE ULTIMATE TRAFFIC CONFIGURATION. WITH THE PROPOSED SIGNALIZATION OPERATIONAL, RE-OPEN THE ROADWAY AND ROUTE TRAFFIC INTO THE PROPOSED PATTERNS. LIQUIDATED DAMAGES OF \$5000.00/HOUR WILL BE ASSESSED UNTIL THESE ITEMS OF WORK ARE COMPLETE.

CONSTRUCTION ITEMS TO BE BUILT DURING THIS PHASE INCLUDE REMAINING SECTIONS OF BARRIER CURB AND GUTTER ALONG THE RAMP ISLANDS, CURB BOX INLETS AND PIPE RUNS LOCATED ON THE MAINLINE SIDE OF THE RAMP ISLANDS, PLACEMENT OF ASPHALT SCRATCH COURSE. TEMPORARY STRIPING DEPICTING THE FINAL STRIPING CONFIGURATION AS SHOWN ON THE ULTIMATE STRIPING PLANS, AND THE ULTIMATE SIGNALIZATION, LIGHTING AND SIGNING FOR THE PROJECT. ALL THE ITEMS LISTED ARE FOR THE INTENDED FINAL NUMBER OF LANES AS SHOWN ON THE PLANS FOR THE ENTIRE PROJECT INCLUDING THE CROSSOVER INTERSECTIONS. THE CONTRACTOR IS ADVISED THAT NO LANE CLOSURES SHALL BE ALLOWED DURING REGULAR TRAFFIC OPERATION HOURS ONCE THE ULTIMATE CROSSOVER INTERCHANGE CONFIGURATION IS OPEN TO TRAFFIC AT THE END OF THE WEEKEND CLOSURE PERIOD.

AFTER THE INTERCHANGE HAS BEEN SWITCHED TO THE ULTIMATE DOUBLE CROSSOVER DIAMOND CONFIGURATION WITH ALL LANES OPEN (ALL LANES OPEN TO TRAFFIC THROUGH THE DCD INTERCHANGE FROM JAMES SANDERS BOULEVARD TO COLEMAN ROAD), WORK CAN THEN PROCEED ON THE REMAINING WORK ITEMS WITHIN THE OUTER ISLANDS AT THE BOTTOM OF RAMPS. REMAINING ITEMS LEFT TO BE BUILT AT THIS STAGE INCLUDE SECTIONS OF SHARED-USED PATH WITHIN THE ISLAND LIMITS, AND FINAL GRADING AND SODDING INSIDE THE ISLANDS.

## PHASE V CONSTRUCTION

FINAL SURFACING FOR THE PROJECT ALONG WITH PERMANENT STRIPING AND PAINTING OF THE CURBS SHALL BE COMPLETED DURING THIS STAGE. THIS WORK SHALL BE PERFORMED USING MOVEABLE LANE CLOSURE OPERATIONS WITH FLAGGERS AND SHALL BE PERFORMED DURING PERIODS OF LOW TRAFFIC FLOW OR AT TIMES APPROVED BY THE ENGINEER. IF TEMPORARY STRIPING IS NECESSARY DURING THESE OPERATIONS OR AT ANY TIME WORK CEASES, IT SHALL BE NOT BE MEASURED FOR PAYMENT. NO TEMPORARY STRIPING WILL BE ALLOWED ON FINAL SURFACES UNLESS IT IS PLACED IN THE FINAL STRIPING CONFIGURATION.

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DATE PLOTTED: November 30, 2018  
FILE NAME: V:\1785\ACTIVE\178565008\TRANS\PORTATION\DESIGN\DRAWING\PLAN\_SHEETS\MOT\_NOTES.DGN

E-SHEET NAME:  
MicroStation v8.11.9.832

# MAINTENANCE OF TRAFFIC NOTES

COUNTY OF	ITEM NO.	SHEET NO.
McCRACKEN	1-154.00	R71

REVISED 11-30-18

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